

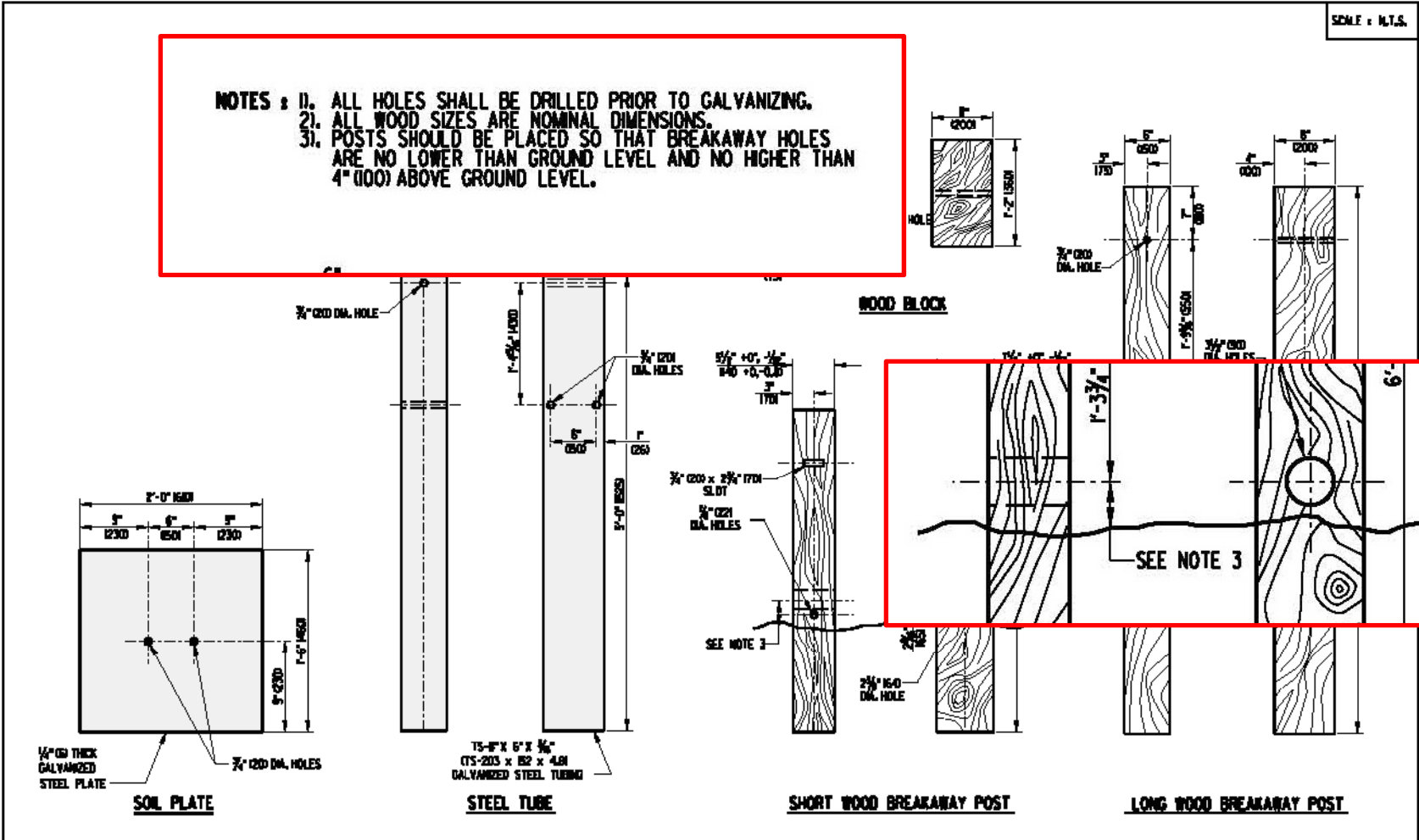
2008 STANDARD CONSTRUCTION DETAILS REVISION OVERVIEW

Billy Sweeney
Quality Section

B-13, Sheet 7 of 13 - Hardware

SCALE: N.T.S.

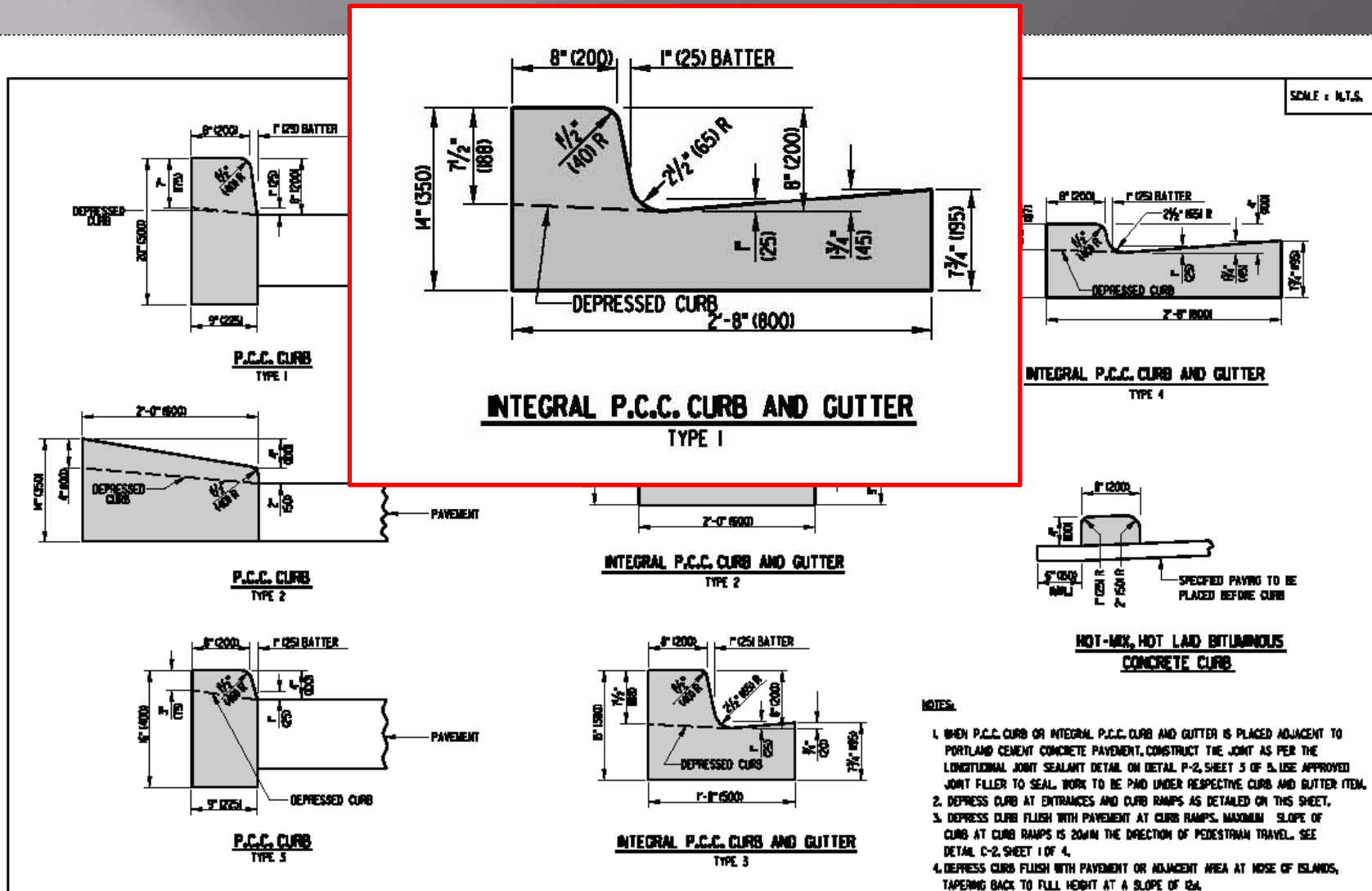
NOTES : 1. ALL HOLES SHALL BE DRILLED PRIOR TO GALVANIZING.
 2. ALL WOOD SIZES ARE NOMINAL DIMENSIONS.
 3. POSTS SHOULD BE PLACED SO THAT BREAKAWAY HOLES ARE NO LOWER THAN GROUND LEVEL AND NO HIGHER THAN 4" (00) ABOVE GROUND LEVEL.



DELAWARE DEPARTMENT OF TRANSPORTATION	HARDWARE			APPROVED
	STANDARD NO. B-13 0000	SHT. 7	OF 13	RECOMMENDED

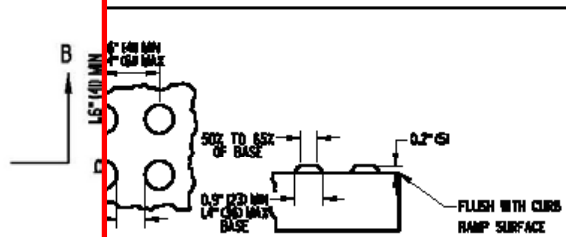
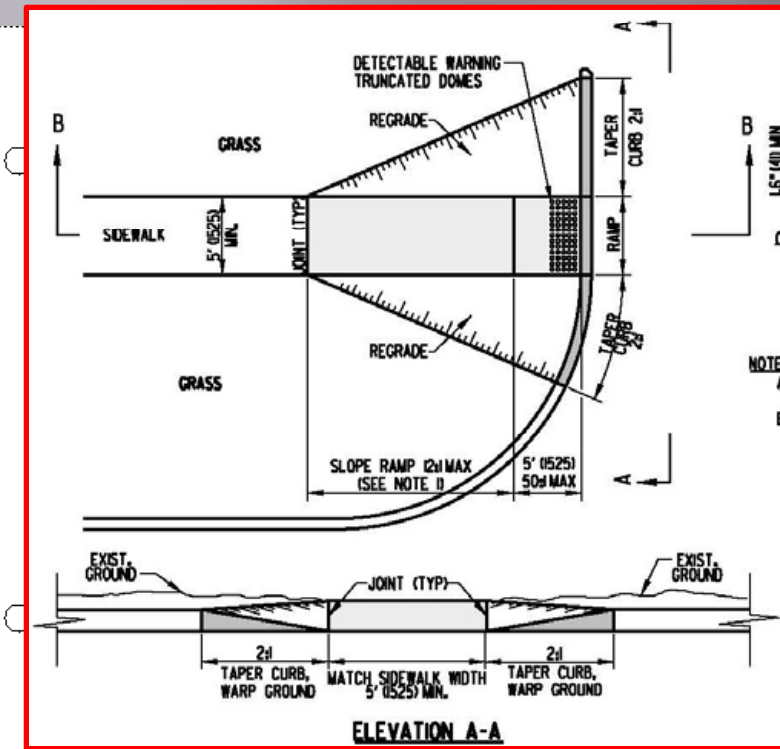
07/14/2008

C-1, Sheet 1 of 1 - P.C.C. Curb, P.C.C. Curb & Gutter, and Hot-Mix Curb



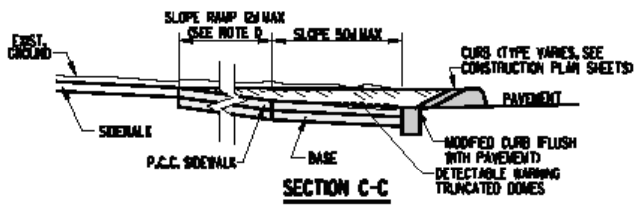
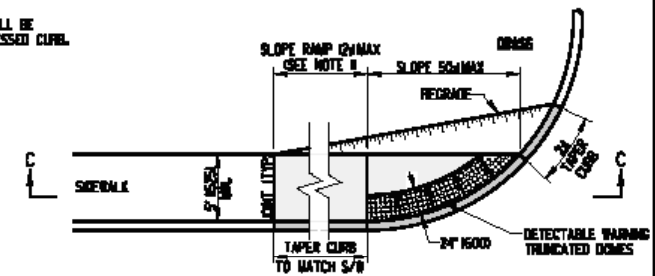
<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	<p>P.C.C. CURB, P.C.C. CURB & GUTTER, AND HOT-MIX CURB</p>			<p>APPROVED</p> <p><i>[Signature]</i> DATE: 1/18/08</p>
	<p>STANDARD NO. C-1 0000</p>	<p>SHT. 1</p>	<p>OF 1</p>	<p>RECOMMENDED</p> <p><i>[Signature]</i> DATE: 1/17/08</p>

C-2, Sheet 1 of 4 - Curb Ramp, Type 1 and Sections



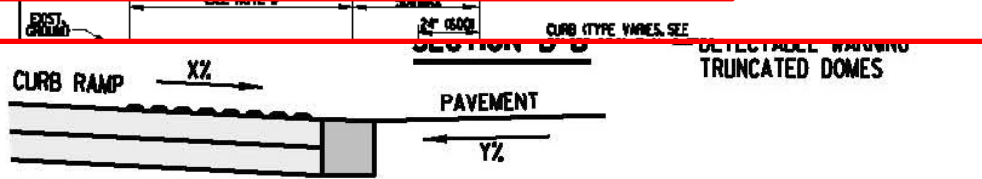
DETECTABLE WARNING TRUNCATED DOME DETAILS

NOTE: ALL DETECTABLE WARNING TRUNCATED DOMES SHALL BE 150mm HIGH AND THE FULL WIDTH OF THE RAMP OR DEPRESSION CURB. SPECIFICATION FOR ADDITIONAL INFORMATION.





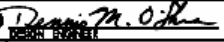
NOTES

1. WHERE A 12% MAXIMUM SLOPE RAMP WILL NOT MEET THE SIDEWALK GRADE WITHIN A LENGTH OF 15' (4575) DUE TO STEEP ADJACENT ROADWAY, THE RAMP LENGTH MAY BE LIMITED TO 15' (4575) AND ALLOWED TO EXCEED 12%.
2. RAMP AND SIDEWALK CROSS SLOPE SHALL BE 5% (125) MAXIMUM.
3. IF GRADING WILL BE STEEPER THAN 6%, THEN A TYPE I CURB OR RETAINING WALL SHOULD BE USED TO ELIMINATE THE NEED FOR THE STEEP SLOPE.
4. THE MAXIMUM DIFFERENCE IN GRADE BETWEEN THE CURB RAMP OR MODIFIED CURB AND THE PAVEMENT SHALL BE 0.2, HOWEVER 0.2 IS PREFERRED.

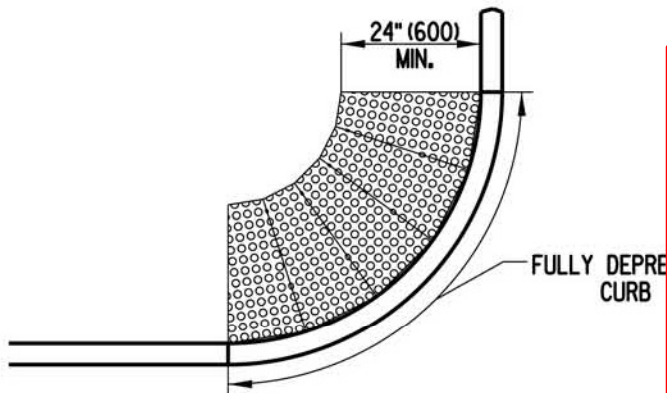
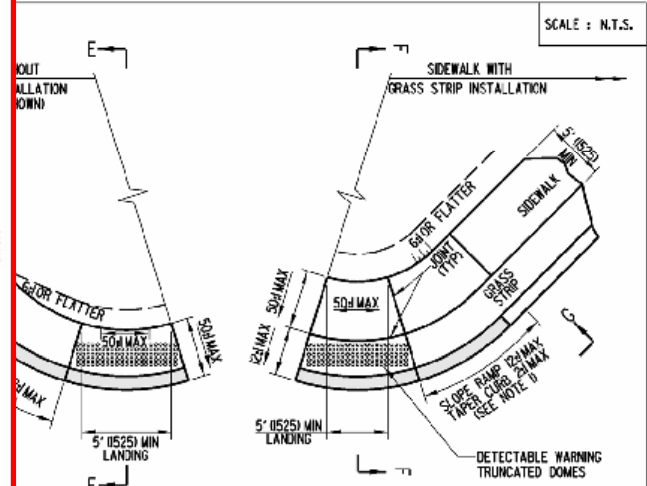
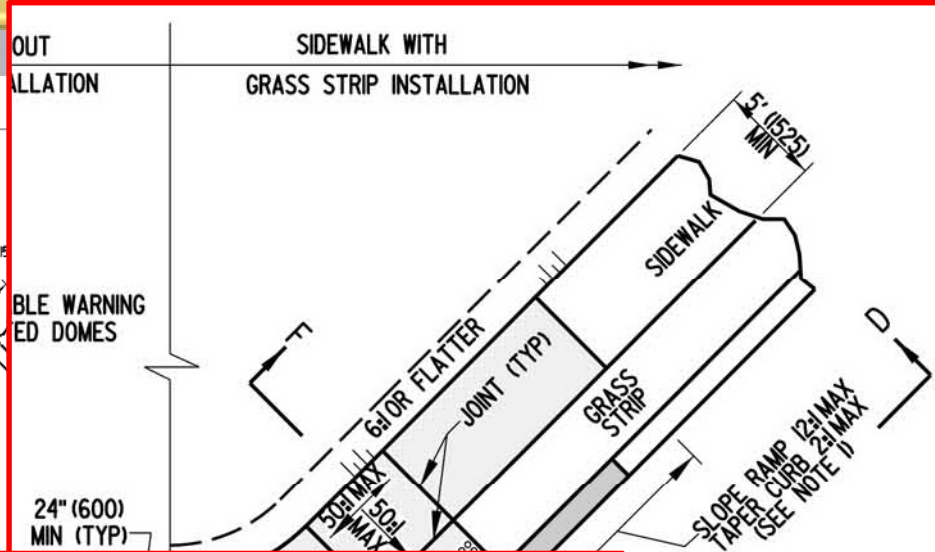


MAXIMUM DIFFERENCE IN GRADE

FOR EXAMPLE, IF THE CURB RAMP AND DEPRESSION CURB SLOPE (X) IS 8.0% AND THE PAVEMENT SLOPE (Y) IS 4.0%, THEN TO DETERMINE THE DIFFERENCE IN GRADE, ADD X + Y TO GET 12.0%, WHICH IS GREATER THAN THE 12% PREFERRED BUT LESS THAN THE 13% MAXIMUM.

 DELAWARE DEPARTMENT OF TRANSPORTATION	CURB RAMP, TYPE 1 AND SECTIONS			APPROVED  <small>DATE</small> 11/16/08
	STANDARD NO. C-3 0909	SHT. 1	OF 4	RECOMMENDED  <small>DATE</small> 11/16/08

C-2, Sheet 2 of 4 - Curb Ramps, Types 2, 3, & 4



SAMPLE LAYOUT OF DETECTABLE WARNING TRUNCATED DOMES ALONG A CURB RADIUS

DETECTABLE WARNINGS SHALL BE PLACED THE FULL WIDTH OF THE DEPRESSED CURB.

NOTES:

- 1). WHERE A 12:1 MAXIMUM SLOPE RAMP WILL NOT MEET THE SIDEWALK GRADE WITHIN A LENGTH OF 15' (4570) DUE TO STEEP ADJACENT ROADWAY, THE RAMP LENGTH MAY BE LIMITED TO 15' (4570), AND THE RAMP SLOPE ALLOWED TO EXCEED 12:1.
- 2). TRANSITION TO EXISTING SIDEWALK WIDTH OVER THE LENGTH OF THE RAMP.
- 3). RAMP AND SIDEWALK CROSS SLOPE SHALL BE 50:1 (2%) MAXIMUM.
- 4). IF GRADING WILL BE STEEPER THAN 6:1 ADJACENT TO THE CURB RAMP OR SIDEWALK, THEN A TYPE I CURB OR RETAINING WALL SHOULD BE USED TO ELIMINATE THE NEED FOR THE STEEP SLOPE.
- 5). FOR THE CURB RAMP, TYPE 3, IF THE WIDTH OF THE FULLY DEPRESSED CURB AT THE STREET IS MORE THAN 5' (1525), THE DETECTABLE WARNING TRUNCATED DOMES SHALL FOLLOW THE RADIUS OF THE CURB CONTINUOUSLY WITHOUT GAPS FOR THE ENTIRE LENGTH OF DEPRESSED CURB.
- 6). THE MAXIMUM DIFFERENCE IN GRADE BETWEEN THE SIDEWALK OR CURB AND THE PAVEMENT SHALL BE 13%, HOWEVER 11% IS PREFERRED. SEE STANDARD NO. C-2, SHEET 1 OF 4.
- 7). IF THE WIDTH OF THE FULLY DEPRESSED CURVED CURB AT THE STREET IS 5' (1525) OR LESS, THEN A RECTANGULAR PIECE OF DETECTABLE WARNING TRUNCATED DOMES MAY BE USED.



DELAWARE
DEPARTMENT OF TRANSPORTATION

CURB RAMPS, TYPES 2, 3, & 4

STANDARD NO. C-2 (2008)

SHT. 2

OF 4

APPROVED

[Signature]
CHIEF ENGINEER

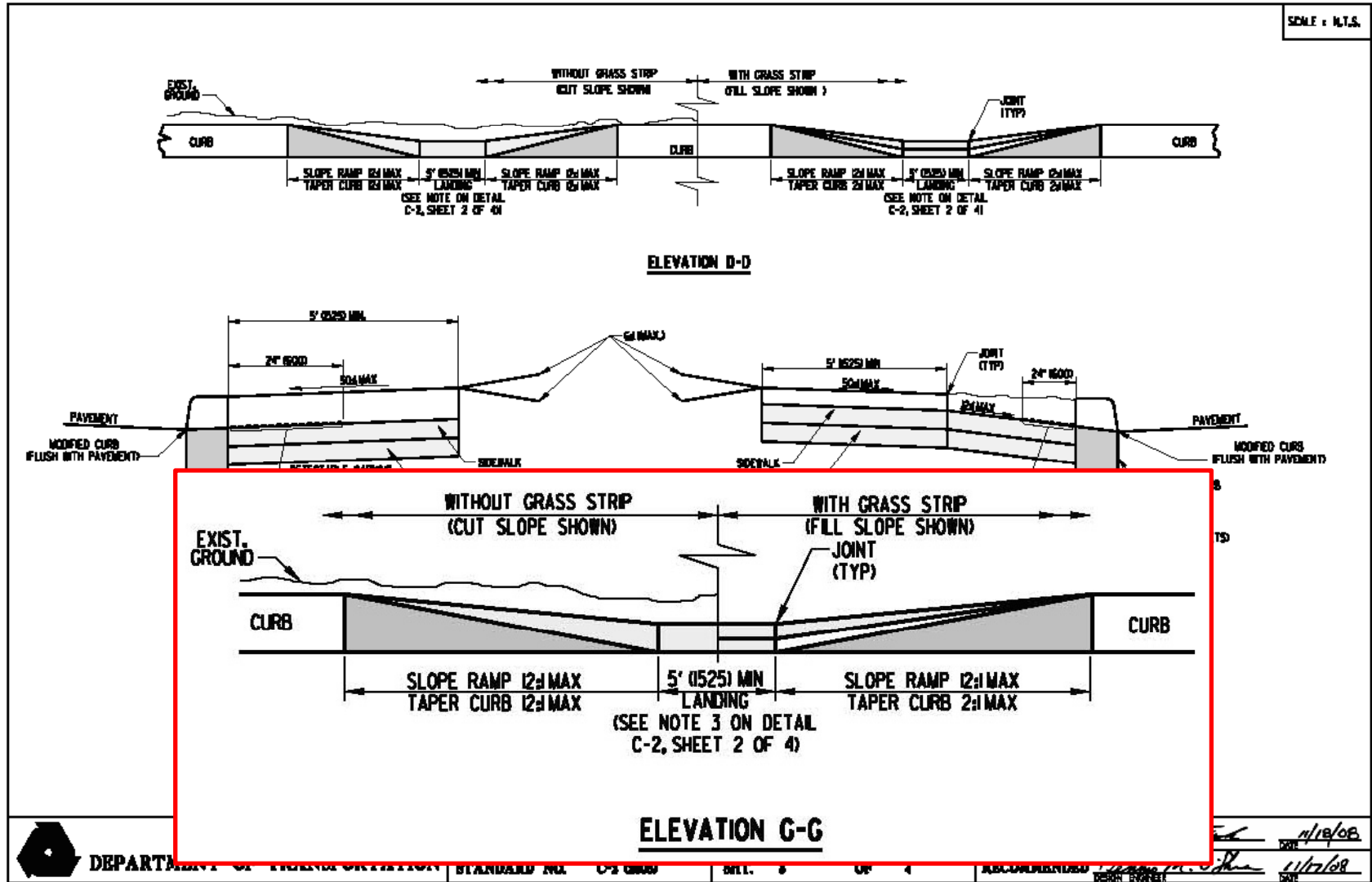
11/18/08
DATE

RECOMMENDED

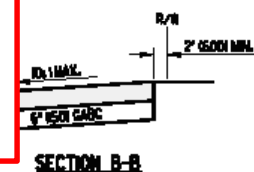
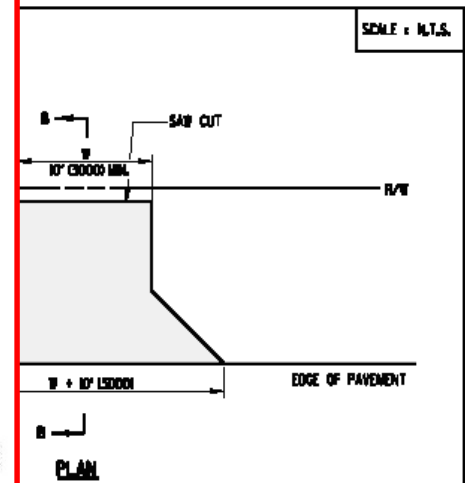
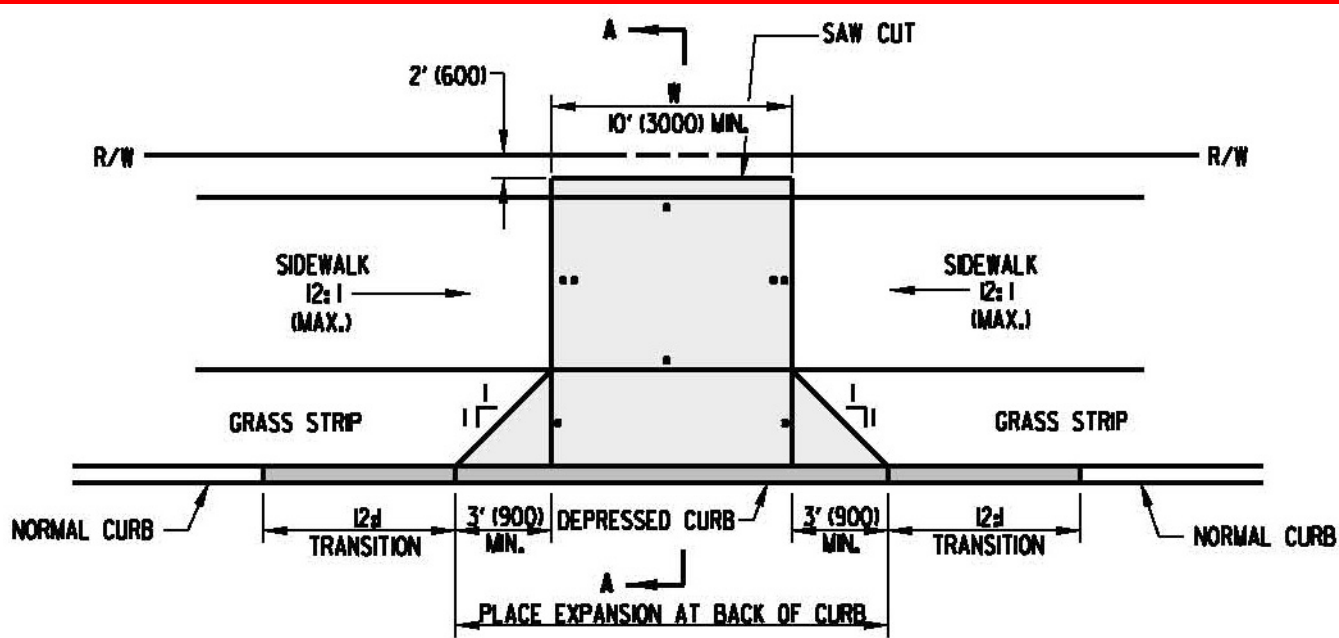
[Signature]
CHIEF ENGINEER

11/17/08
DATE

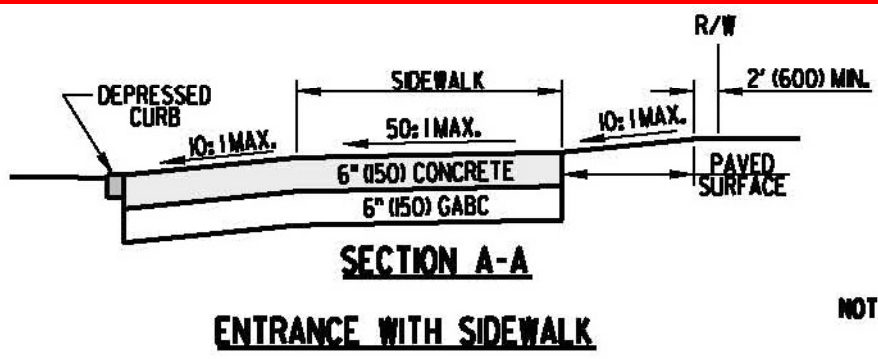
C-2, Sheet 3 of 4 - Curb Ramp Sections for Types 2 & 3



C-3, Sheet 1 of 1 - Entrances



ENTRANCE WITHOUT SIDEWALK



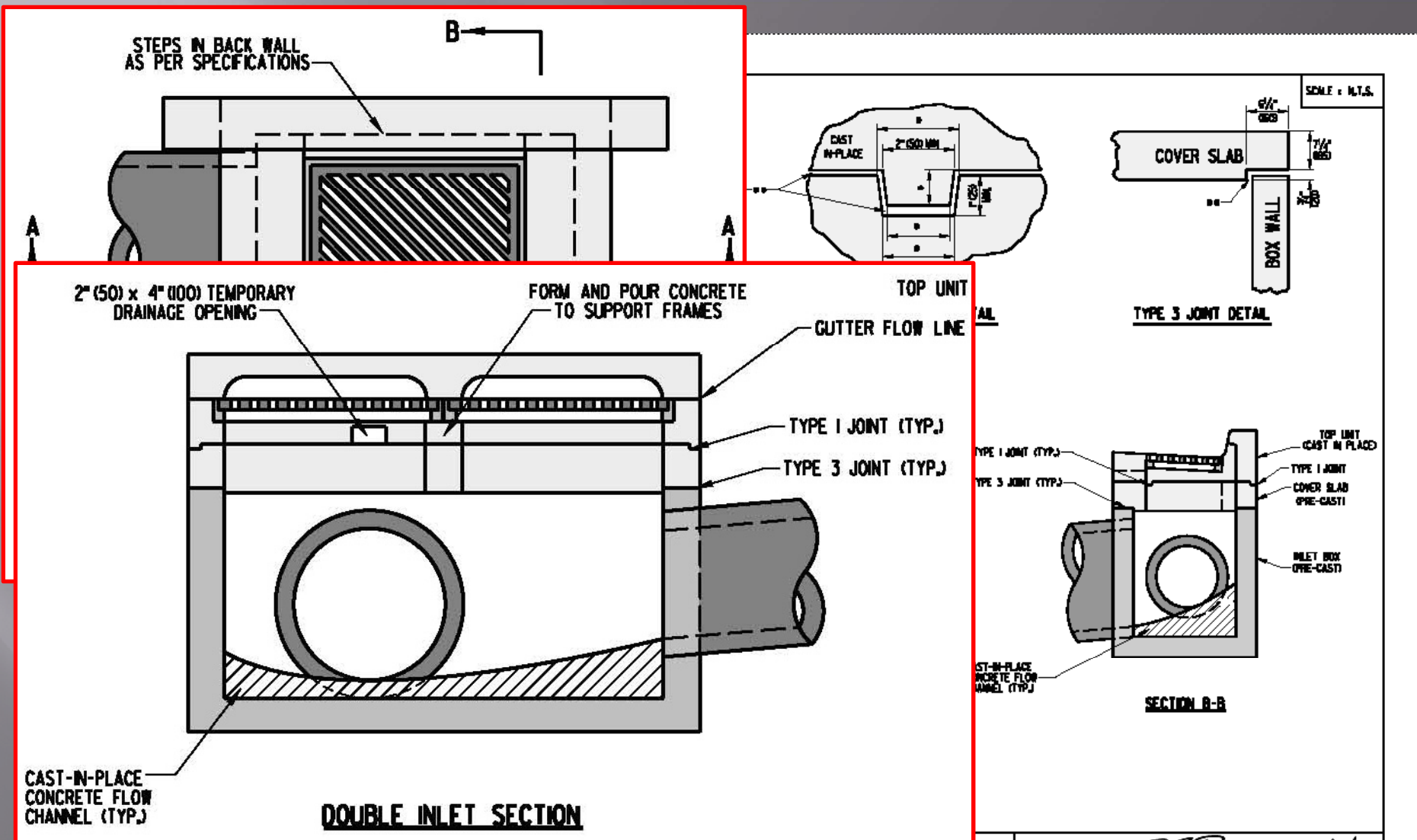
ENTRANCE WITH SIDEWALK

10:1 OR GREATER, THE M FLARE CAN BE OMITTED.

NOT

DEPARTMENT OF TRANSPORTATION	STANDARD NO. C-3 0000	SHT. 1 OF 1	APPROVED	DATE 11/16/08
			RECOMMENDED	DATE 11/16/08

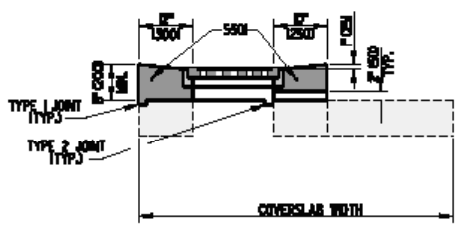
D-5, Sheet 1 of 9 - Drainage Inlet Assembly



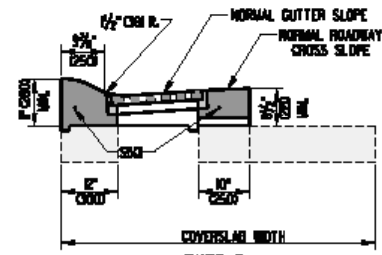
DELAWARE DEPARTMENT OF TRANSPORTATION	DRAINAGE INLET ASSEMBLY			APPROVED	<i>[Signature]</i>	DATE	11/16/08		
	STANDARD NO.	D-5 000	SHT.	1	OF	9	RECOMMENDED	<i>[Signature]</i>	DATE

D-5, Sheet 3 of 9 - Drainage Inlet Top Units

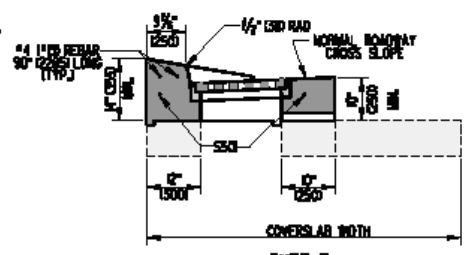
SCALE = N.T.S.



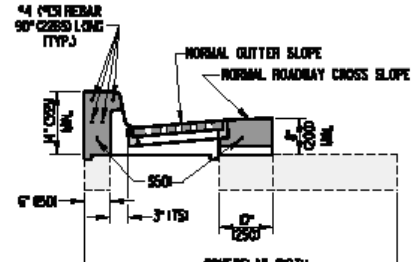
TYPE A



TYPE D

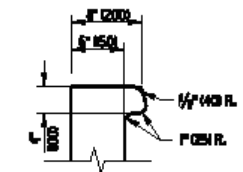


TYPE E

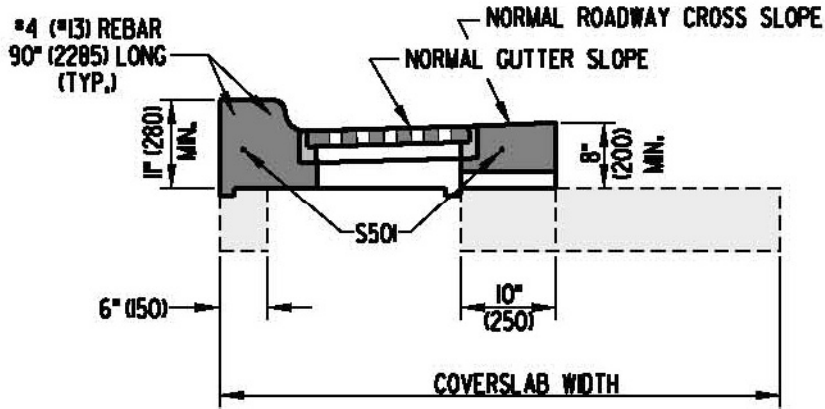


TYPE C

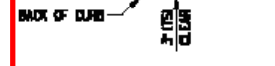
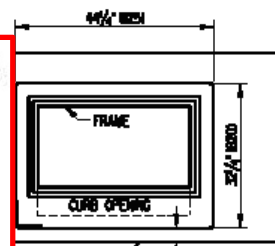
INLET TOP UNIT APPLICATIONS	
TOP UNIT	CURB
TYPE A	USE IN DRAINAGE SHALE
TYPE B	INTERNAL PCC CURB & GUTTER, TYPE 1 & 3, PCC CURB TYPE 1
TYPE C	INTERNAL PCC CURB & GUTTER, TYPE 4, PCC CURB TYPE 3
TYPE D	INTERNAL PCC CURB & GUTTER, TYPE 2
TYPE E	PCC CURB TYPE 2



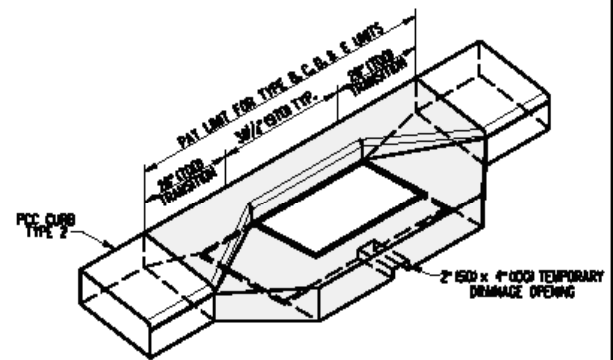
CURB OPENING DETAIL



TYPE C



S501 BENDING DIAGRAM
 S501 MUST BE ONE CONTINUOUS BAR. IF MORE THAN ONE, THERE MUST BE A 12\"/>



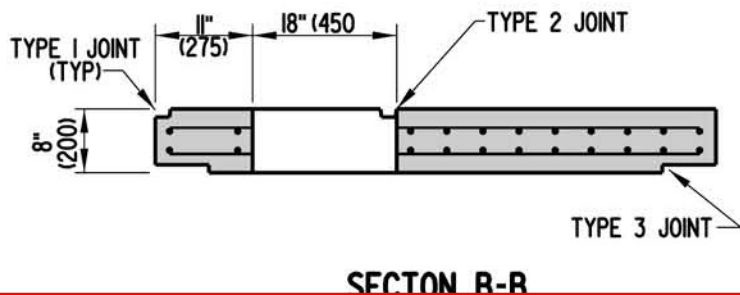
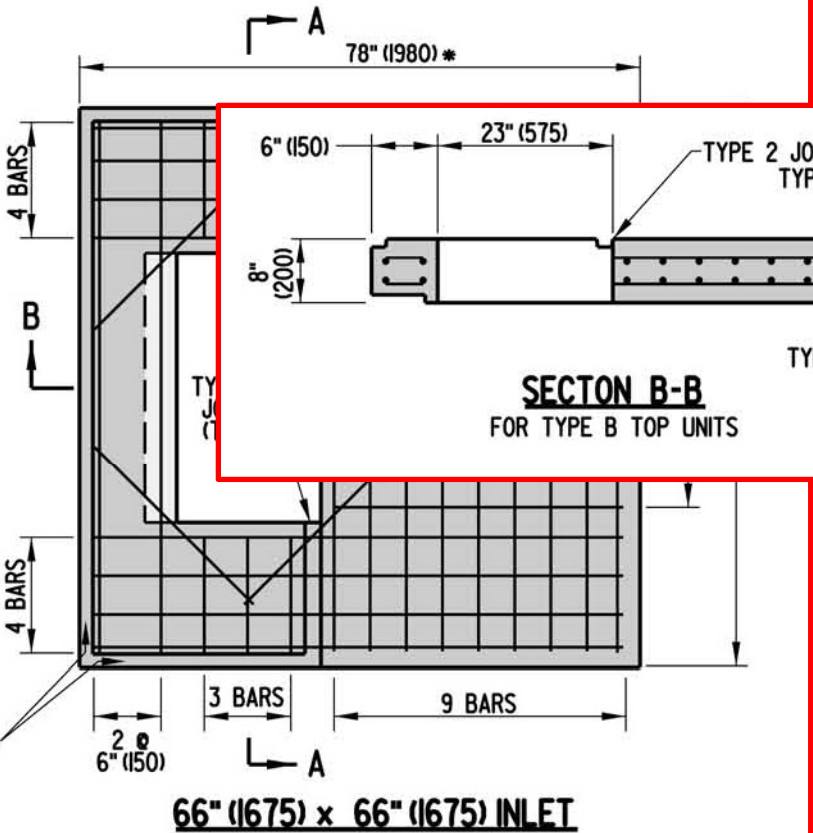
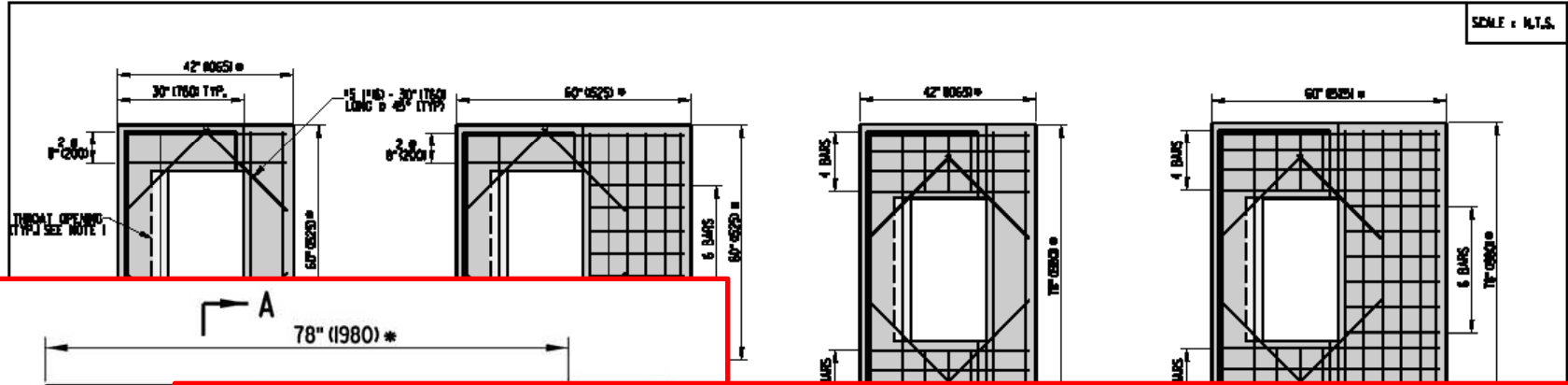
ISOMETRIC VIEW
 TYPE E UNIT SHOWN

DRAINAGE INLET TOP UNITS
 D-5 CMB SHT. 3 OF 9

APPROVED [Signature] 11/16/08
RECOMMENDED [Signature] 11/16/08

D-5, Sheet 4 of 9 - Drainage Inlet Cover Slab Details

SCALE = N.T.S.



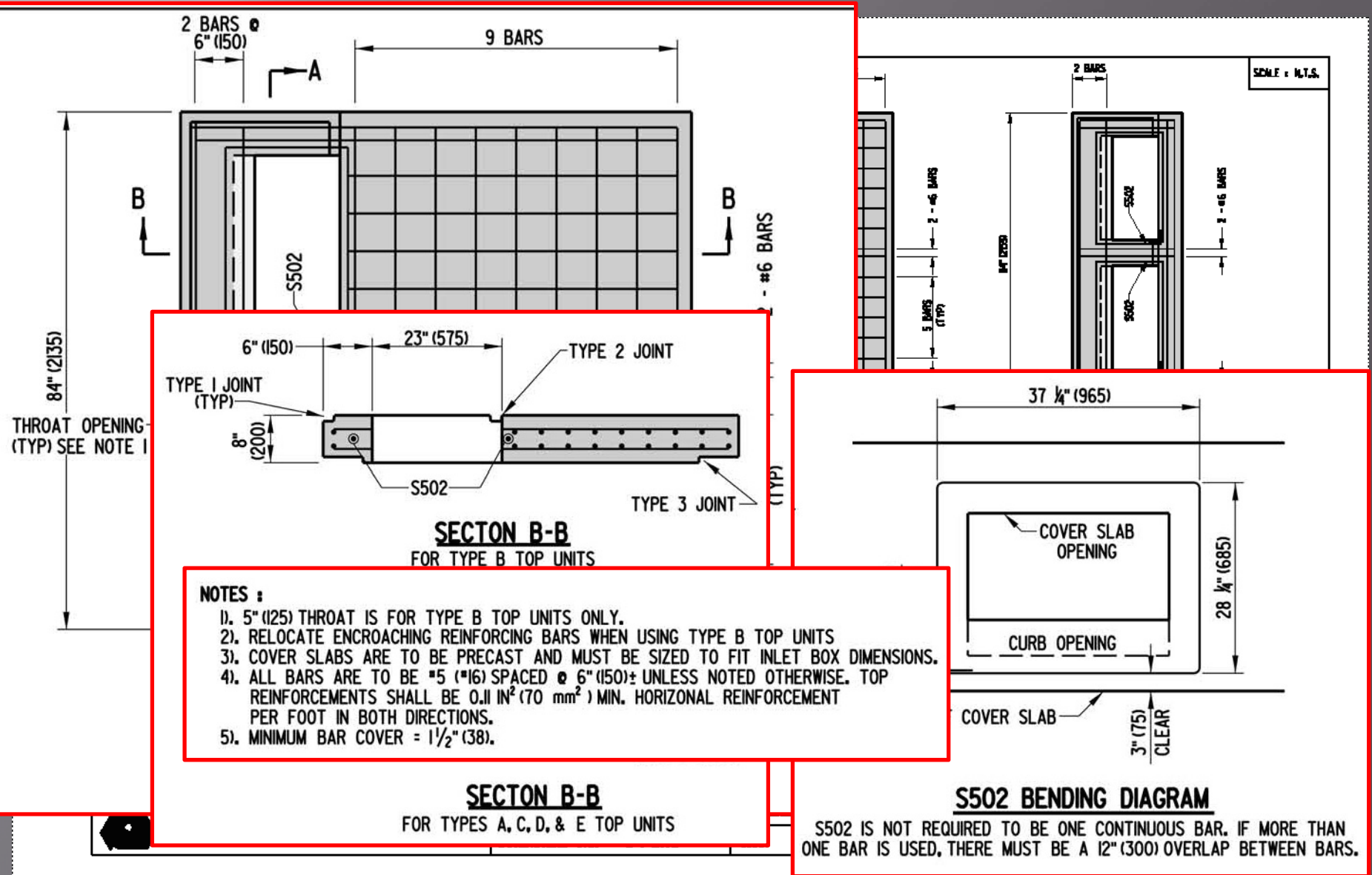
- NOTES :**
- 1). 5" (125) THROAT IS FOR TYPE B TOP UNIT ONLY.
 - 2). RELOCATE ENCROACHING REINFORCING BARS WHEN USING TYPE B UNIT.
 - 3). COVER SLABS SHALL BE PRE-CAST AND MUST BE SIZED TO FIT INLET BOX DIMENSIONS.
 - 4). ALL BARS ARE TO BE #5 (#16) SPACED @ 6" (150) ± UNLESS NOTED OTHERWISE. TOP REINFORCEMENT SHALL BE 0.11 IN² (70 mm²) HORIZONTAL REINFORCEMENT PER FOOT IN BOTH DIRECTIONS.
 - 5). MINIMUM BAR COVER = 1/2" (38)
- * - DIMENSIONS TO MATCH OUTSIDE TO OUTSIDE DIMENSIONS OF BOX.

66" (1675) x 66" (1675) INLET

INLET	SHT. 4 OF 9	RECOMMENDED	11/7/08
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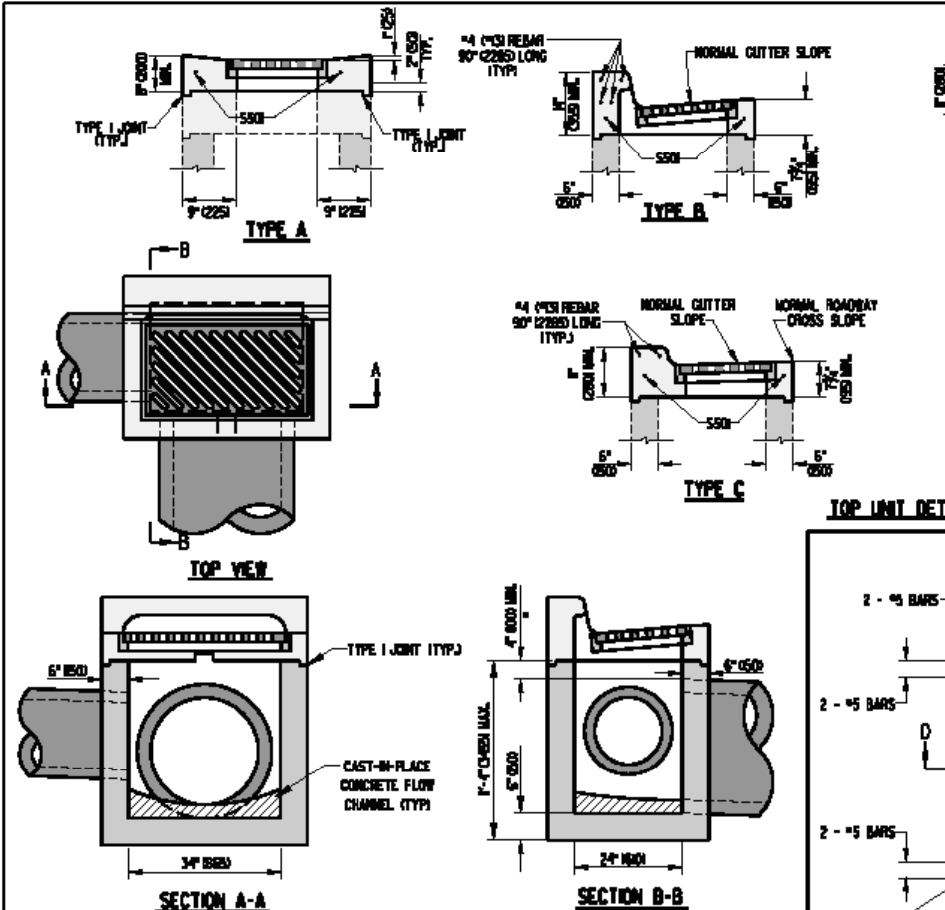
10/16/2008

D-5, Sheet 5 of 9 - Double Inlet Cover Slab Details



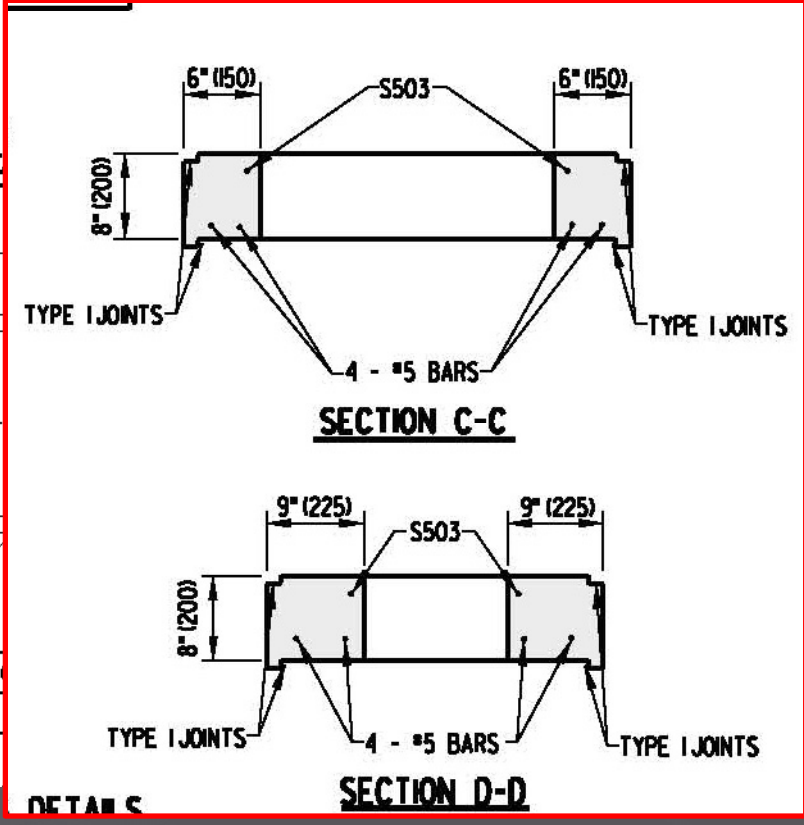
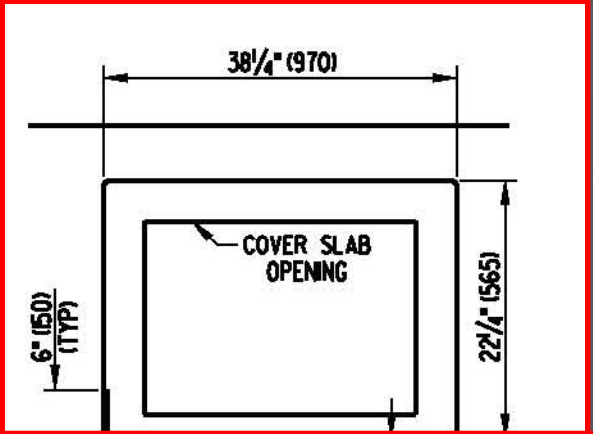
S502 IS NOT REQUIRED TO BE ONE CONTINUOUS BAR. IF MORE THAN ONE BAR IS USED, THERE MUST BE A 12" (300) OVERLAP BETWEEN BARS.

D-5, Sheet 6 of 9 - 34" (865) x 24" (610) Drainage Inlet and Cover Slab Details



DRAINAGE INLET DETAILS
NOTE: REFER TO PREVIOUS SHEETS FOR REINFORCING REQUIREMENTS
-- SEE OPTIONAL PIPE OPENING DETAIL ON STANDARD NO. D-4, SHEET 1 OF 1

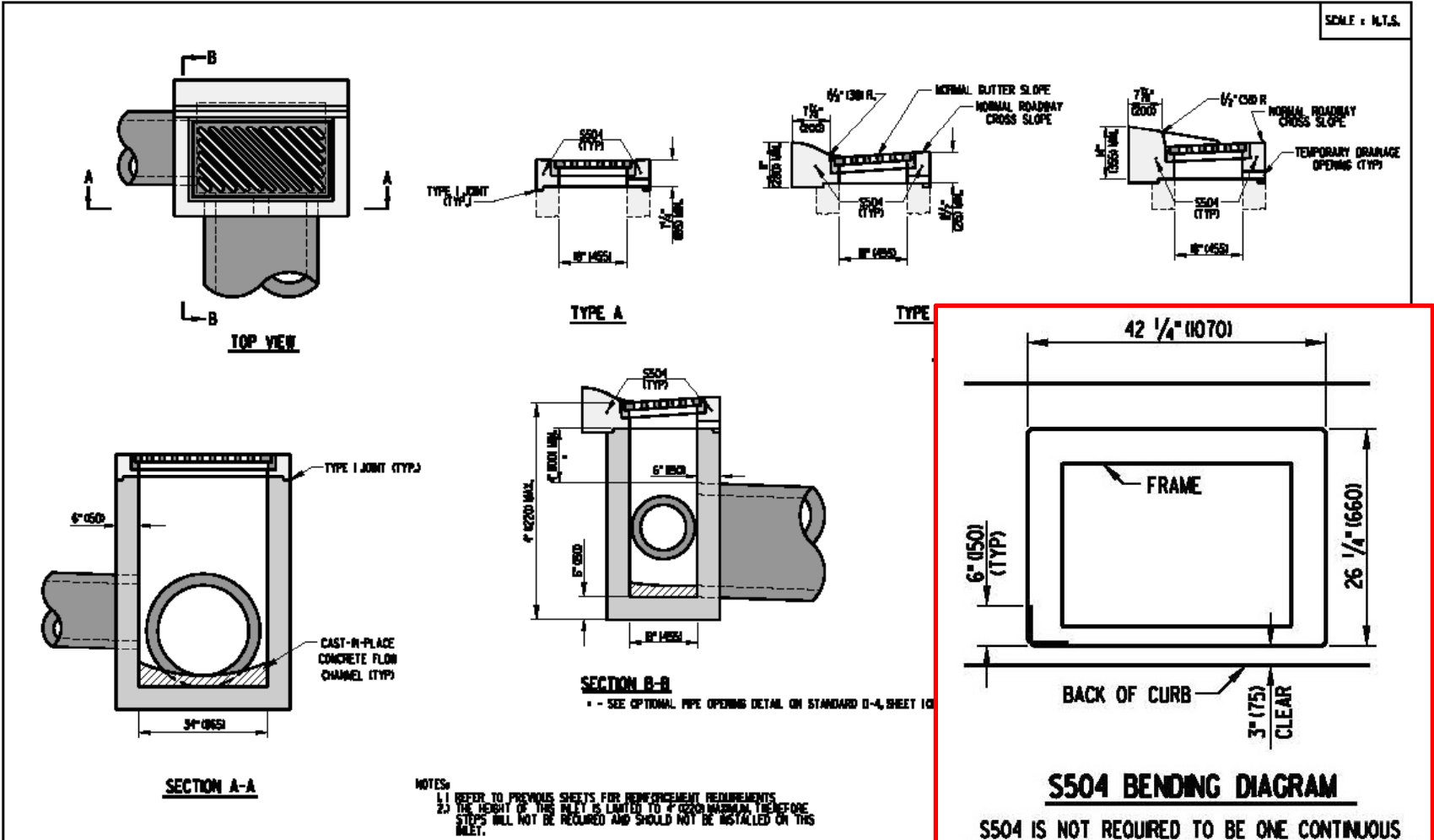
DELAWARE DEPARTMENT OF TRANSPORTATION	34" O.D. x 24" C.I.D. DRAINAGE INLET AND COVER	
	STANDARD NO. D-5 O.M.D.	SHT.



DETAILS


D-5, Sheet 7 of 9 - 34" (865) x 18" (455) Drainage Inlet Details

SCALE : N.T.S.



- NOTES:**
- 1. REFER TO PREVIOUS SHEETS FOR REINFORCEMENT REQUIREMENTS.
 - 2. THE HEIGHT OF THIS INLET IS LIMITED TO 4 FEET MAXIMUM. THEREFORE STEPS WILL NOT BE REQUIRED AND SHOULD NOT BE INSTALLED ON THIS INLET.

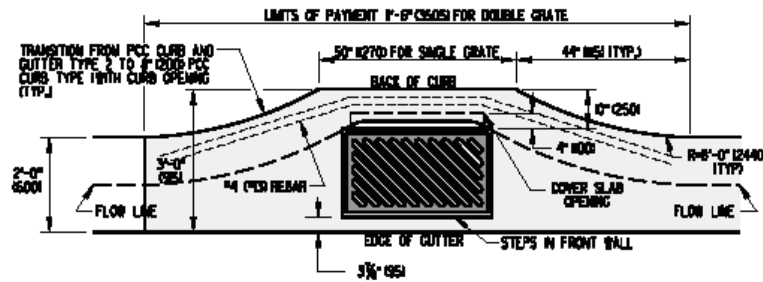
S504 IS NOT REQUIRED TO BE ONE CONTINUOUS BAR. IF MORE THAN ONE BAR IS USED, THERE MUST BE A 12" (300) OVERLAP BETWEEN BARS.

 **DELAWARE
DEPARTMENT OF TRANSPORTATION**

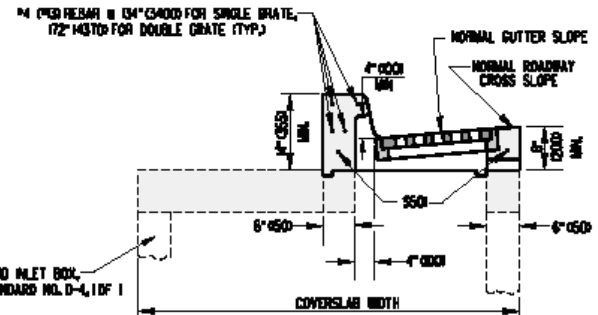
34" (865) x 18" (455) DRAINAGE INLET DETAILS		
STANDARD NO. D-4 OMS	SHT. 7	OF 9

D-5, Sheet 8 of 9 - Subdivision Inlet Details

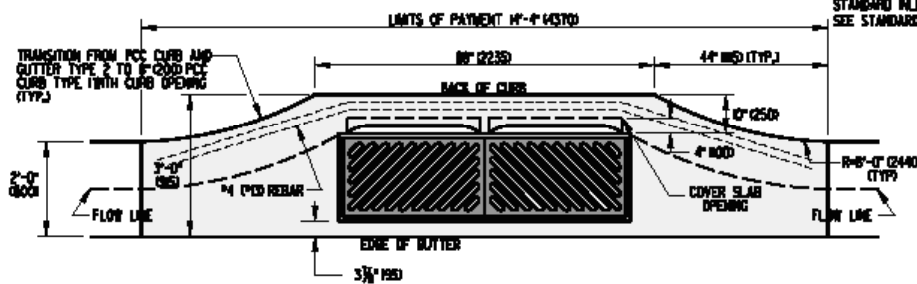
SCALE: N.T.S.



SINGLE GRATE SETUP



SUBDIVISION TOP & CONFIGURATION



DOUBLE GRATE SETUP

- NOTES:**
1. MINIMUM BOX SIZE TO BE 3'-0" (914) X 2'-0" (600).
 2. PIPE OPENINGS IN THE FRONT WALL SHALL NOT INTERFERE WITH THE STEPS. THE PIPE SHALL BE SHIFTED HORIZONTALLY TO AVOID THE STEPS. IT MAY BE NECESSARY TO USE A LARGER BOX TO AVOID CONFLICT BETWEEN STEPS AND PIPE OPENING.
 3. SEE DETAIL D-5, SHEET 5 OF 9 FOR S&W BAR DIAGRAM.



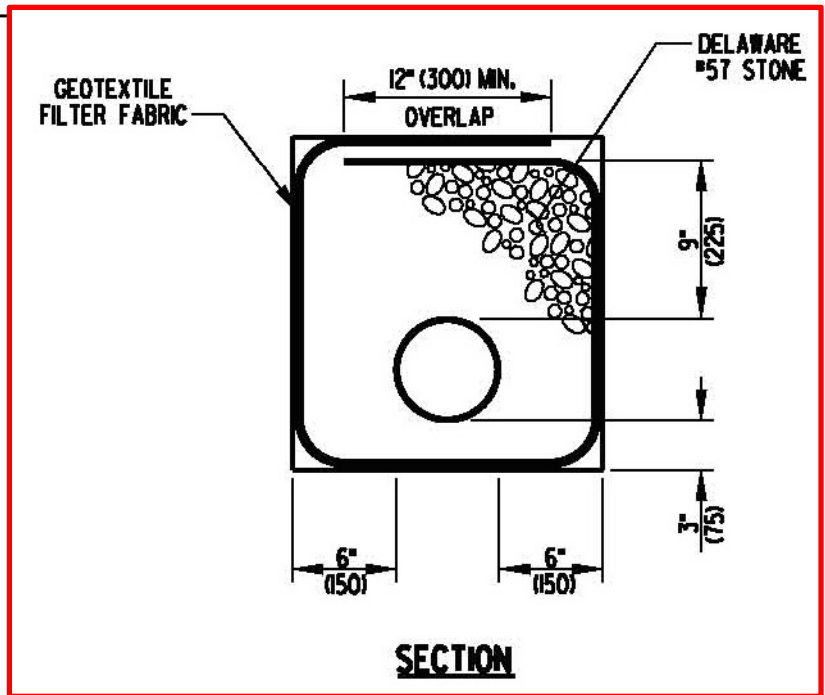
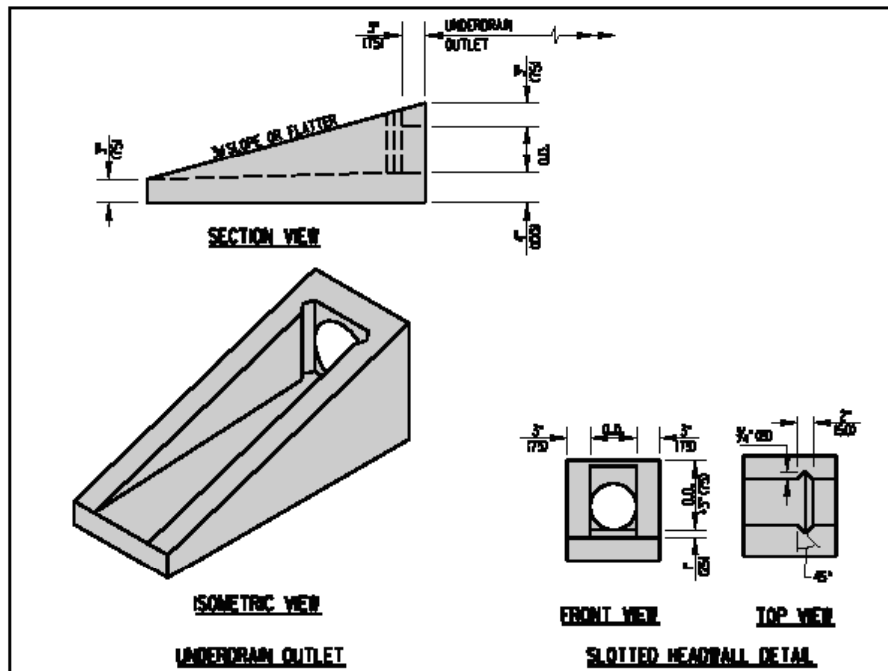
**DELAWARE
DEPARTMENT OF TRANSPORTATION**

SUBDIVISION INLET DETAILS

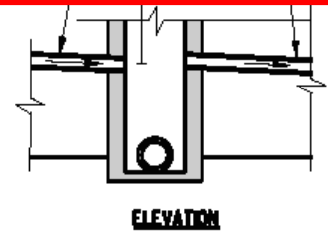
STANDARD NO. **D-4 000** SHT. **8** OF **9**

APPROVED *[Signature]* DATE **11/16/08**
RECOMMENDED *[Signature]* DATE **11/16/08**

D-9, Sheet 1 of 1 - Perforated Pipe Underdrain Detail

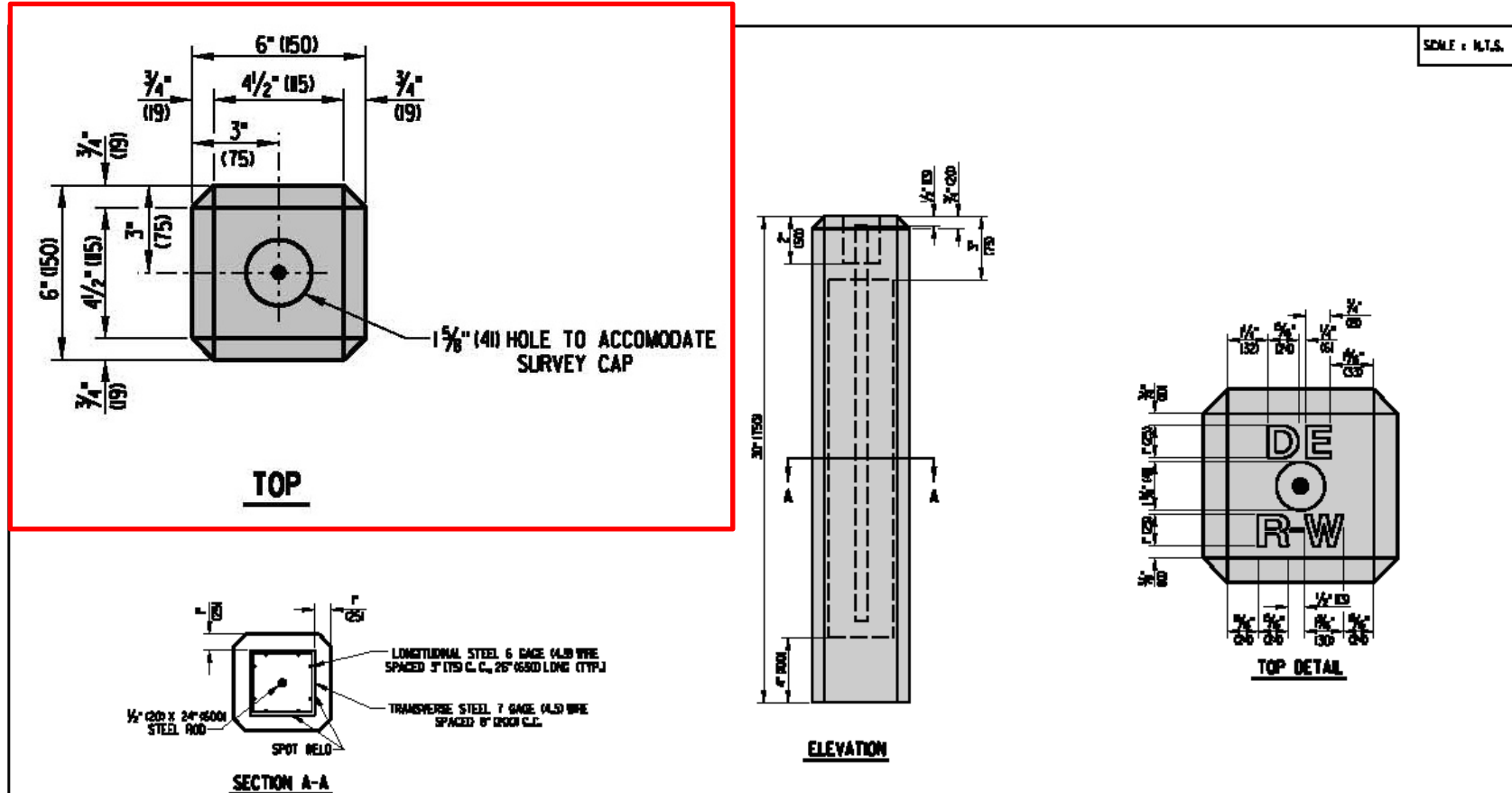


- NOTES:**
1. THE PERFORATED PIPE UNDERDRAIN SHALL BE LOCATED AS SHOWN ON THE TYPICAL SECTIONS OF THE CONSTRUCTION PLANS.
 2. GEOTEXTILE FILTER FABRIC SHALL BE PLACED ENTIRELY OVER THE TOP OF UNDERDRAIN TRENCH AND LAPPED AS SHOWN.
 3. SLOPE OF UNDERDRAINS SHALL MATCH ROADWAY GRADE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
 4. OUTLET PIPE CONFIGURATIONS SHALL USE 45 DEGREE ELBOWS OR SHALL USE STRAIGHT PIPE WITH A MINIMUM RADIUS OF 3' (900) TO DIRECT UNDERDRAIN PIPE INTO SIDE OF DRAINAGE INLET OR TO POSITIVE GRADE. PIPE SHALL ALSO BE NON-PERFORATED AND HAVE A SMOOTH INTERIOR.
 5. RODENT SCREEN SHALL SMOOTHLY FIT THE PROVIDED SLOT WITH THE SCREEN LIP FITTING TIGHT TO THE BOTTOM FLOW LINE.
 6. A 4" (100) FLEXIBLE DELINEATOR SHALL BE FURNISHED AND INSTALLED AT THE DIRECTION OF THE ENGINEER TO MARK THE LOCATION OF THE CONCRETE HEADWALL.
 7. WHEN TWO LINES OF PIPE UNDERDRAIN DRAIN TO A LOW POINT, EACH PIPE MUST HAVE ITS OWN OUTLET.
 8. PERFORATED PIPE UNDERDRAIN SHALL NOT BE PLACED UNDER GUARDRAIL IN ORDER TO AVOID PUNCTURING.



DELAWARE DEPARTMENT OF TRANSPORTATION	PERFORATED PIPE UNDERDRAIN DETAIL		APPROVED <i>[Signature]</i> 11/16/08 <small>DATE</small>
	STANDARD NO. D-9 0800	SHT. 1 OF 1	RECOMMENDED <i>[Signature]</i> 11/16/08 <small>DATE</small>

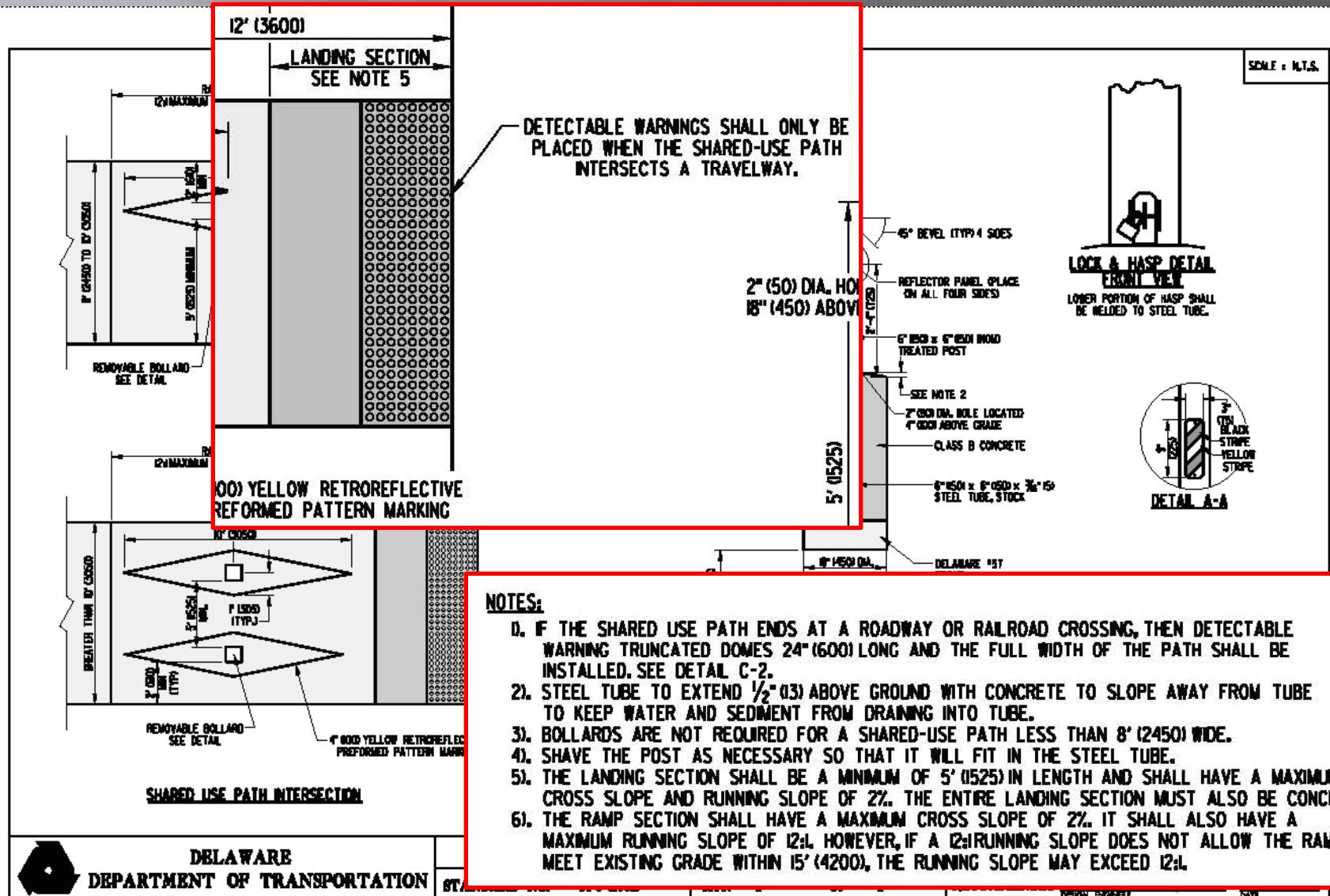
M-2, Sheet 1 of 1 - Concrete Monument



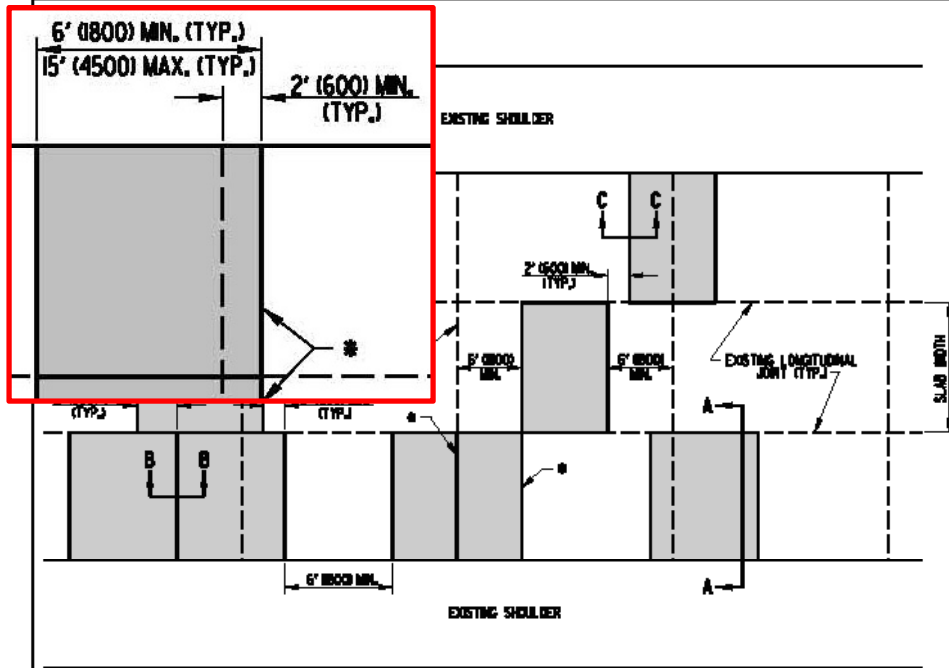
NOTES : 1. LONGITUDINAL STEEL SHALL BE HELD IN PLACE BY CRADLES.
 2. LETTERS TO BE COUNTERSUNK IN TOP OF MARKER 1/2" (12.5)

DELAWARE DEPARTMENT OF TRANSPORTATION	CONCRETE MONUMENT			APPROVED <small>DATE</small> 11/16/08
	STANDARD NO. M-2 0000	SHT. 1 OF 1	RECOMMENDED <small>DATE</small> 11/16/08	

M-3, Sheet 1 of 1 - Bollard & Shared-Use Path Details



P-2, Sheet 1 of 5 - P.C.C. Pavement Patching



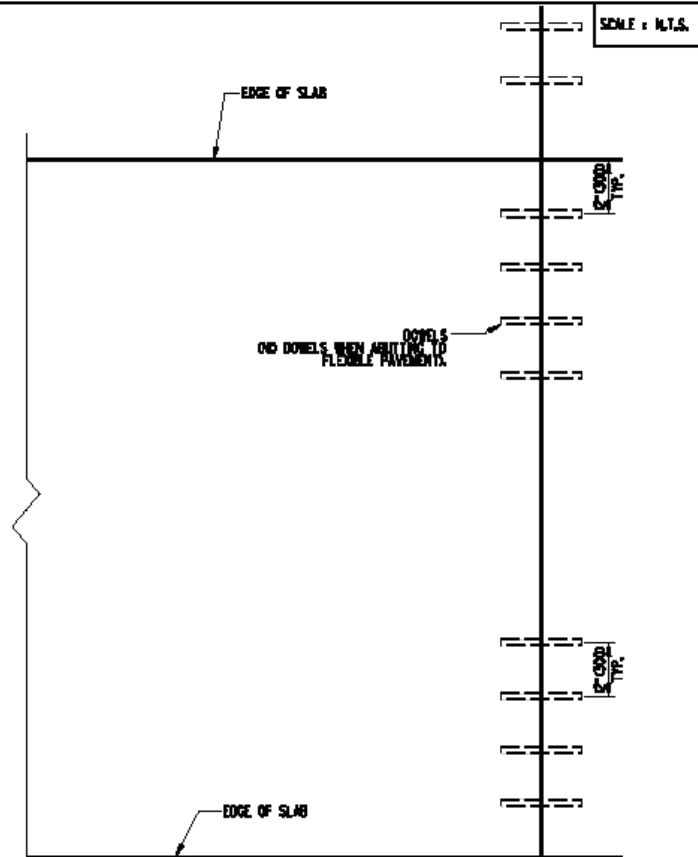
PLAN

* - PROPOSED LOCATIONS FOR TRANSVERSE JOINTS SHALL EXACTLY MATCH THE ALIGNMENT OF THE FINAL EXISTING OR RELOCATED TRANSVERSE JOINTS IN ALL IMMEDIATELY ADJACENT LANES.




NOTES:

1. WHEN REPAIRING EXISTING TRANSVERSE JOINTS, THE PATCH SHALL EXTEND A MINIMUM OF 2' (600) THROUGH THE EXISTING JOINT, WHICH WILL RELOCATE THE JOINT.
2. PROPOSED LOCATIONS FOR TRANSVERSE JOINTS, WHEN NOT ALIGNED WITH THE FINAL EXPECTED TRANSVERSE JOINT LOCATIONS IN THE IMMEDIATELY ADJACENT LANES, SHALL BE OFFSET A MINIMUM OF 2' (600) FROM THE AFFORMENTIONED JOINTS.
3. THE LONGITUDINAL JOINT ALIGNMENT SHALL BE STRAIGHT AND CONTINUOUS THROUGH THE REPAIRED AREA.

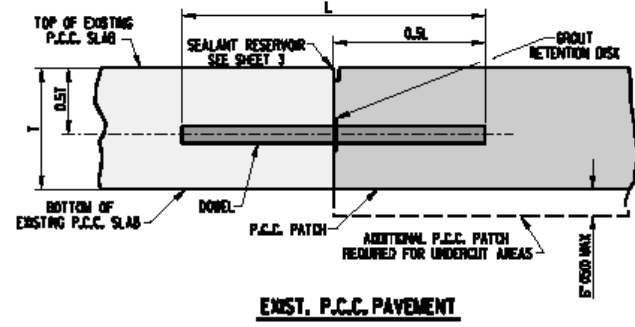
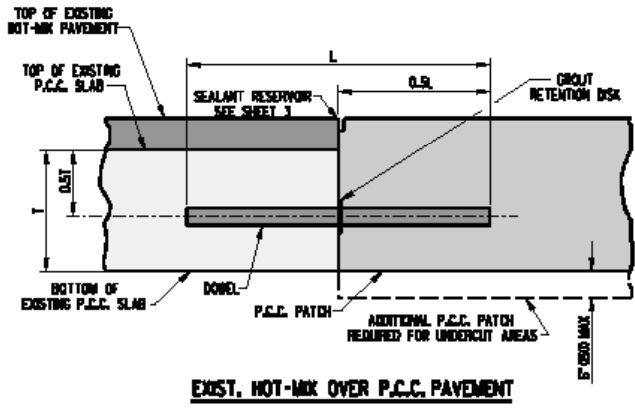
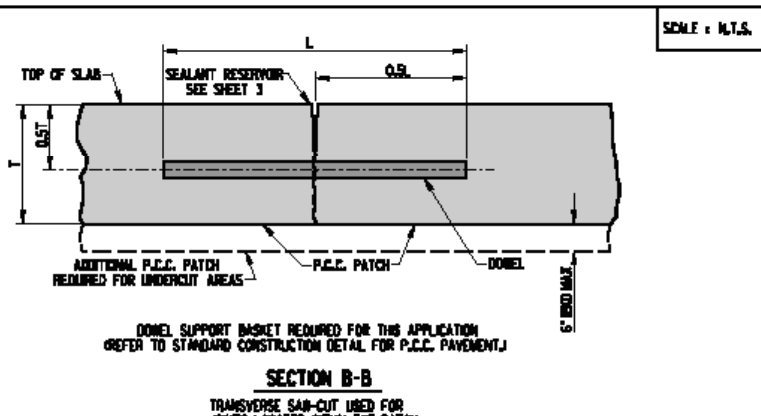
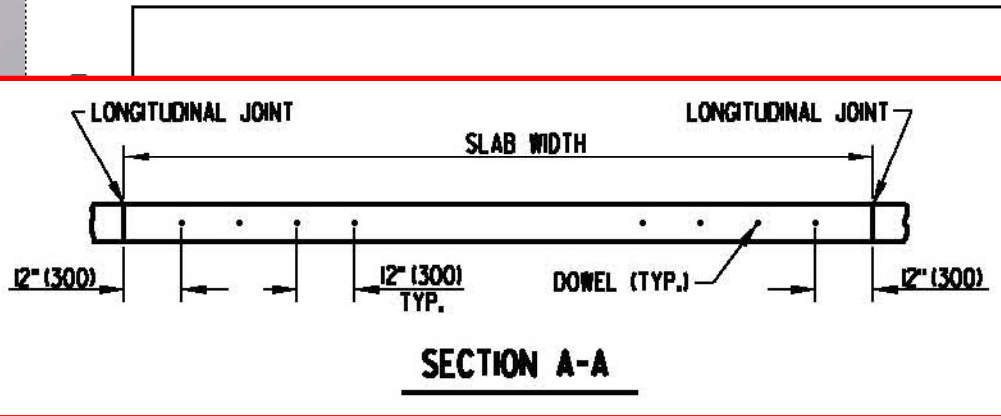
FULL DEPTH PATCH



SLAB PLAN WITH DOWEL AND TIE LOCATIONS

 DELAWARE DEPARTMENT OF TRANSPORTATION	P.C.C. PAVEMENT PATCHING		APPROVED
	STANDARD NO. P-2 CMB	SHT. 1 OF 5	 4/18/08 <small>DATE</small>
			RECOMMENDED  4/17/08 <small>DATE</small>

P-2, Sheet 2 of 5 - P.C.C. Pavement Patching



SECTION C-C
TRANSVERSE CONSTRUCTION JOINT USED ON JOINTS BETWEEN EXISTING PAVEMENT AND PATCH

FULL DEPTH PATCH

<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	P.C.C. PAVEMENT PATCHING			APPROVED <small>DATE</small> 11/14/08
	STANDARD NO. P-2 090	SHT. 2	OF 5	RECOMMENDED <small>DATE</small> 11/17/08

Directions to Standard Details on Website

The screenshot shows the Delaware Department of Transportation website. The browser address bar is highlighted with a red box, showing the URL <http://www.delDOT.gov/>. The website header includes the Delaware state logo and navigation links for Governor, General Assembly, Courts, Elected Officials, and State Agencies. A search bar is located below the header.

The main content area features a navigation bar with three tabs: "Wicks Sworn In" (active), "Building The Bridge", and "Registration Reminders". The "Wicks Sworn In" tab is highlighted with a blue arrow.

The main article is titled "Wicks continues as Secretary". It includes a photograph of Secretary Carolann Wicks being sworn in by Governor Jack Markell. The text states: "DelDOT Secretary Carolann Wicks was reconfirmed by the Delaware State Senate late January 21, 2009, and then sworn in with Delaware Supreme Court Justice Henry duPont Ridgely, as Governor Jack Markell held the Bible. Secretary Wicks, a Professional Engineer, has worked at DelDOT for more than 26 years, and has been Secretary since February 2006. A University of Delaware graduate, Wicks began her career at DelDOT as an entry-level engineer in 1982. Since then, she has served in various roles as a public servant for three Transportation Secretaries, including as DelDOT's first woman Chief Engineer. As Secretary, she manages a budget of almost \$1 billion and is responsible for 2,600 employees. DelDOT has responsibility for 90 percent of the state's roads and bridges and manages the state transit system (Delaware Transit Corporation) and the Division of Motor Vehicles." A link "Markell Nominates Wicks About the Secretary Accomplishments" is provided.

The left sidebar contains a navigation menu with categories: HOME, SERVICES, INFORMATION, Publications (highlighted with a red box), and Links. The "Publications" section lists: Archaeology, Red Light Enforcement, and Media Gallery.

The right sidebar contains sections: Alerts (Air Quality Forecast, No Warnings), Hot Topics (Summit Bridge Closure, Old Cooch's Bridge Rd, Video Gallery, Save Gas: Ride DART, Subdivision Manual, Figure 1-2, TIS Warrants, Report A Road Condition), Spotlight (Adopt-A-Highway In 2009, WE'RE RECRUITING ENGINEERS!), Subscribe (MyDelDOT, RSS Feeds), and Media Gallery (Workzone Safety Awareness, From March 2008, View Archive).

The bottom of the page features a "Recent News" section with a list of articles from January 2009, a "Traffic Info" section with links to traffic maps and cameras, a "Workshops" section with public hearings and virtual workshops, and a "Weather Info" section with a WTCM 1380 AM logo.

Directions to Standard Details on Website

Delaware Department of Transportation | Publications

HOME
About DeDOT
DeDOT Secretary
Press Releases
Divisions
FAQs
Employment
Public Workshops
Related Links
DMV
DART
EZPass
Contact Information
Office Locations
Agency Site Map

SERVICES
Online Services

INFORMATION
Live Traffic
Interactive Traffic Maps
Biking in Delaware
Community Programs and Services
Doing Business
FOIA Regulations
Projects
Publications
Archaeology
Red Light Enforcement
Media Gallery

Publications

Current Reports

- Delaware Airports & Aviation Economic Impact Assessment Brochure
- Delaware State Aviation System Plan Update (Dec 2008)
- Capital Transportation Program FY 2009 - FY 2014 (10/31/08)
- Bond Bill Committee Hearing (06/23/2008)
- 2007 DeDOT Fact Book
- Bond Bill Presentation FY 2009 (04/28/2008)
- Budget Hearing Presentation FY 2009 (11/07/2007)
- Capital Transportation Program FY 2008 - FY 2013-Final (8/23/07)
- Consultation Process for Non-Metropolitan Locally Elected and Appointed Officials
- Delaware Freight and Goods Movement Plan Executive Summary and Technical Report**
- Delaware Rail-to-Trail & Railways Trail Facility Master Plan - Final**
- DeDOT Road Network Download Files
- Development Related Improvements Requiring New Rights-of-Way
- Joint Finance Committee Presentation FY 2009 (02/19/2008)
- Property Management Report for 2007
- Recessed Pavement Markers Locations

Brochures

- Community Transportation Fund
- Corridor Capacity Preservation Program
- Customer Satisfaction
- Real Estate Advanced Acquisition Approval Process

Maps & Guides Order Forms

- Functional Classification Maps
- Order form for Manuals
- Order a map of Delaware

Manuals

- Bridge Atlas
- Bridge Design Manual
- Bridge Inspection Policies and Procedures
- Bridge Maintenance Manual

Current Reports
• Brochures
• Maps & Guides Order Forms
• Manuals
• Archived Materials

SCROLL
DOWN

Directions to Standard Details on Website

The screenshot shows a web browser window displaying the Delaware Department of Transportation website. The address bar shows the URL: http://www.delDOT.gov/information/pubs_forms/const_details/index.shtml. The page title is "Delaware Department of Transportation | Publications".

The main content area is titled "Publications And Forms" and "Standard Construction Details". Below this title, it states: "The Standard Construction Details files are indexed by year and provided in Adobe Portable Document Format (PDF)".

A list of years is displayed, with the years 2008, 2007, 2006, 2005, 2004, 2002, 2001, 2000, 1999, 1998, and 1997 highlighted by a red rectangular box. Below the list, there is a link for "Approved Alternatives to Standard Details".

The left sidebar contains a navigation menu with sections: HOME, SERVICES, and INFORMATION. The right sidebar contains "Information" and "Public Feedback" sections.

At the bottom of the page, there is a footer with the text: "Last Updated: Wednesday, 28-Jan-2009 12:54:16 Eastern Standard Time" and a navigation bar with links: "site map | about this site | contact us | translate | delaware.gov".

Directions to Standard Details on Website

The screenshot shows a web browser window displaying the Delaware Department of Transportation website. The address bar shows the URL: http://www.delDOT.gov/information/pubs_forms/const_details/2008/index.shtml. The page features a header with the State of Delaware logo and a navigation menu. The main content area is titled "Publications And Forms" and "Standard Construction Details". It includes an "Important Notice" about metric and U.S. customary units, followed by a table of 2008 standards. Below this is a "back to the top" link and a table for "Section I - Barrier" details.

Delaware Department of Transportation | Publications

HOME
 About DelDOT
 DelDOT Secretary
 Press Releases
 Divisions
 FAQs
 Employment
 Public Workshops
 Related Links
 DMV
 DART
 EZPass
 Contact Information
 Office Locations
 Agency Site Map

SERVICES
 Online Services

INFORMATION
 Live Traffic
 Interactive Traffic Maps
 Biking in Delaware
 Community Programs and Services
 Doing Business
 FOIA Regulations
 Projects
 Publications
 Archaeology
 Red Light Enforcement
 Media Gallery

Publications And Forms
 Standard Construction Details

Important Notice
 Design values are presented in this document in both metric and U.S. customary units and were developed independently within each system. The relationship between the metric and U.S. customary values is neither an exact (soft) conversion nor a completely rationalized (hard) conversion. The metric values are those that would have been used had this document been presented exclusively in metric units, The U.S. customary values are those that would have been used if this document had been presented exclusively in U.S. customary units. Therefore, the user is advised to work completely in one system and not attempt to convert directly between the two.

2008	
Sheet No.	Name
DelDOT 2008 Standards	
<i>If smaller file size is required, this file is divided into four sections below:</i>	
	DelDOT 2008 Standards Pages 1-5
	DelDOT 2008 Standards Pages 36-70
	DelDOT 2008 Standards Pages 71-105
	DelDOT 2008 Standards Pages 106-150
2008 Standards Revisions	
	Index of Sheets - 1
	Index of Sheets - 2
	Index of Sheets - 3
	Index of Sheets - 4
	Index of Sheets - 5

[back to the top](#)

Section I - Barrier	
Sheet No.	Name
B-L (2001)	Barrier Legend
B-1	Guardrail Applications
B-1 (2004) - 1	Plans - (Type 1, Type 2, and Type 3)
B-1 (2004) - 2	Elevations and Splice Detail

Information

- Main Index
- 2008
- 2007
- 2006
- 2005
- 2004
- 2002
- 2001
- 2000
- 1999
- 1998
- 1997
- Pre-1997

Adobe Acrobat Reader is required to view files saved in Adobe Portable Document Format (PDF). A free download is available from Adobe Systems. Visitors with visual impairments or limitations can also access a free suite of tools from Adobe Systems to assist in viewing documents in PDF format.

Public Feedback

Contact: Public Relations
 Phone: 302-760-2080
 1-800-652-5600 (in-state only)
 email: dotpr@state.de.us

2008 STANDARD CONSTRUCTION DETAILS REVISION OVERVIEW

Questions or comments??

If you have any questions or comments please contact:

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Quality Section