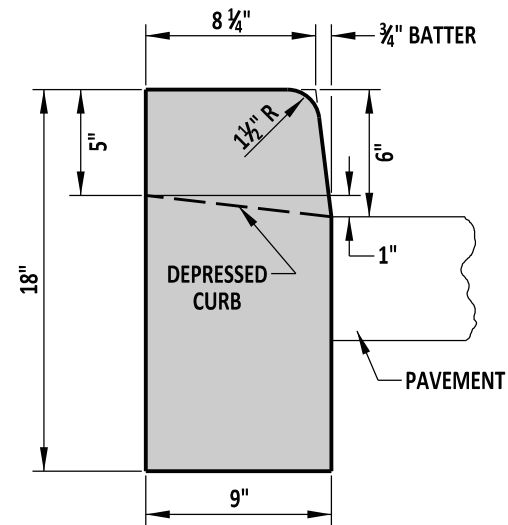
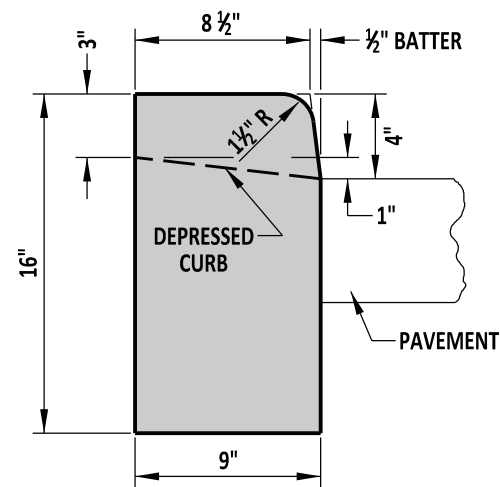


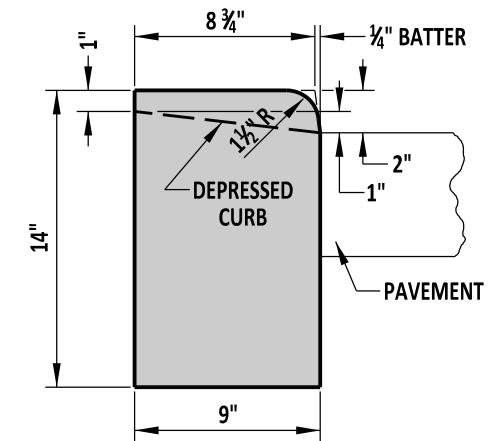
**P.C.C. CURB**  
TYPE 1-8



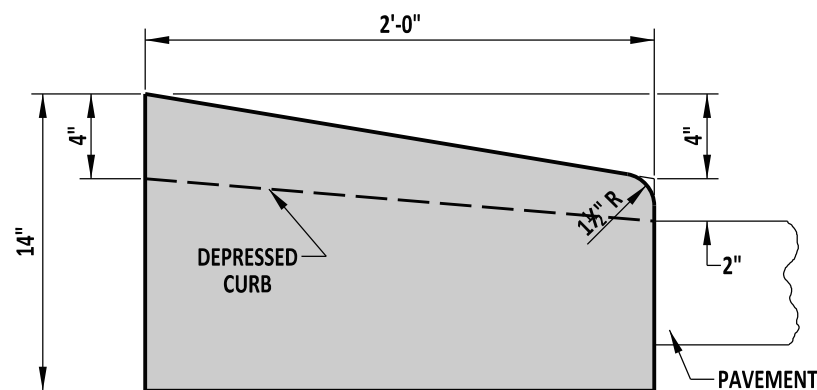
**P.C.C. CURB**  
TYPE 1-6



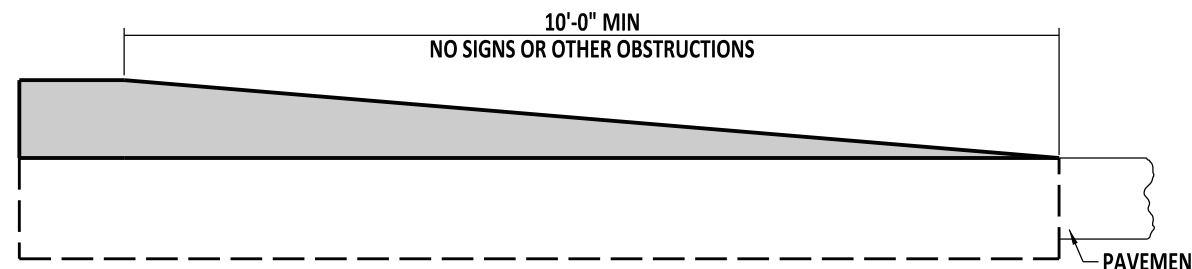
**P.C.C. CURB**  
TYPE 1-4



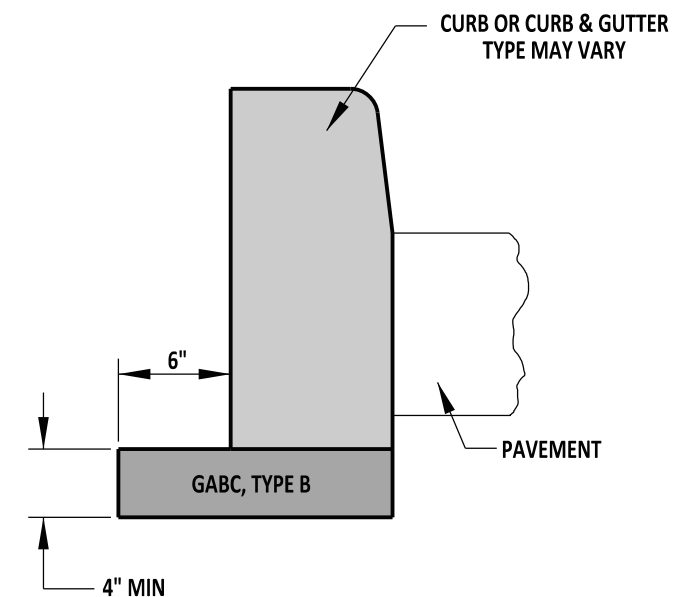
**P.C.C. CURB**  
TYPE 1-2



**P.C.C. CURB**  
TYPE 2



**TYPICAL TAPER SECTION**  
**AT NOSE OF MEDIANS**  
TYPE 1-8 CURB SHOWN



**TYPICAL CURB SECTION**

**NOTES:**

- 1). WHEN P.C.C. CURB OR INTEGRAL P.C.C. CURB AND GUTTER IS PLACED ADJACENT TO PORTLAND CEMENT CONCRETE PAVEMENT, CONSTRUCT THE JOINT AS PER THE LONGITUDINAL JOINT SEALANT DETAIL ON DETAIL P-2, SHEET 3 OF 5. USE APPROVED JOINT FILLER TO SEAL. WORK TO BE PAID UNDER RESPECTIVE CURB AND GUTTER ITEM.
- 2). DEPRESS CURB AT ENTRANCES AND CURB RAMPS AS DETAILED ON THIS SHEET.
- 3). DEPRESS CURB FLUSH WITH PAVEMENT AT CURB RAMPS. MAXIMUM SLOPE OF CURB AT CURB RAMPS IS 20:1 IN THE DIRECTION OF PEDESTRIAN TRAVEL. SEE DETAIL C-2, SHEET 1 OF 4.
- 4). DEPRESS CURB FLUSH WITH PAVEMENT OR ADJACENT AREA AT LEADING EDGE OF TRIANGULAR ISLANDS, TAPERING BACK TO FULL HEIGHT AT A SLOPE OF 4:1.



**DELAWARE**  
**DEPARTMENT OF TRANSPORTATION**

P.C.C. CURB		APPROVED	SIGNATURE ON FILE <small>CHIEF ENGINEER</small>	12/22/2011 <small>DATE</small>
STANDARD NO.	C-1 (2011)	SHT. 1 OF 2	RECOMMENDED	SIGNATURE ON FILE <small>DESIGN ENGINEER</small>
				12/21/2011 <small>DATE</small>