


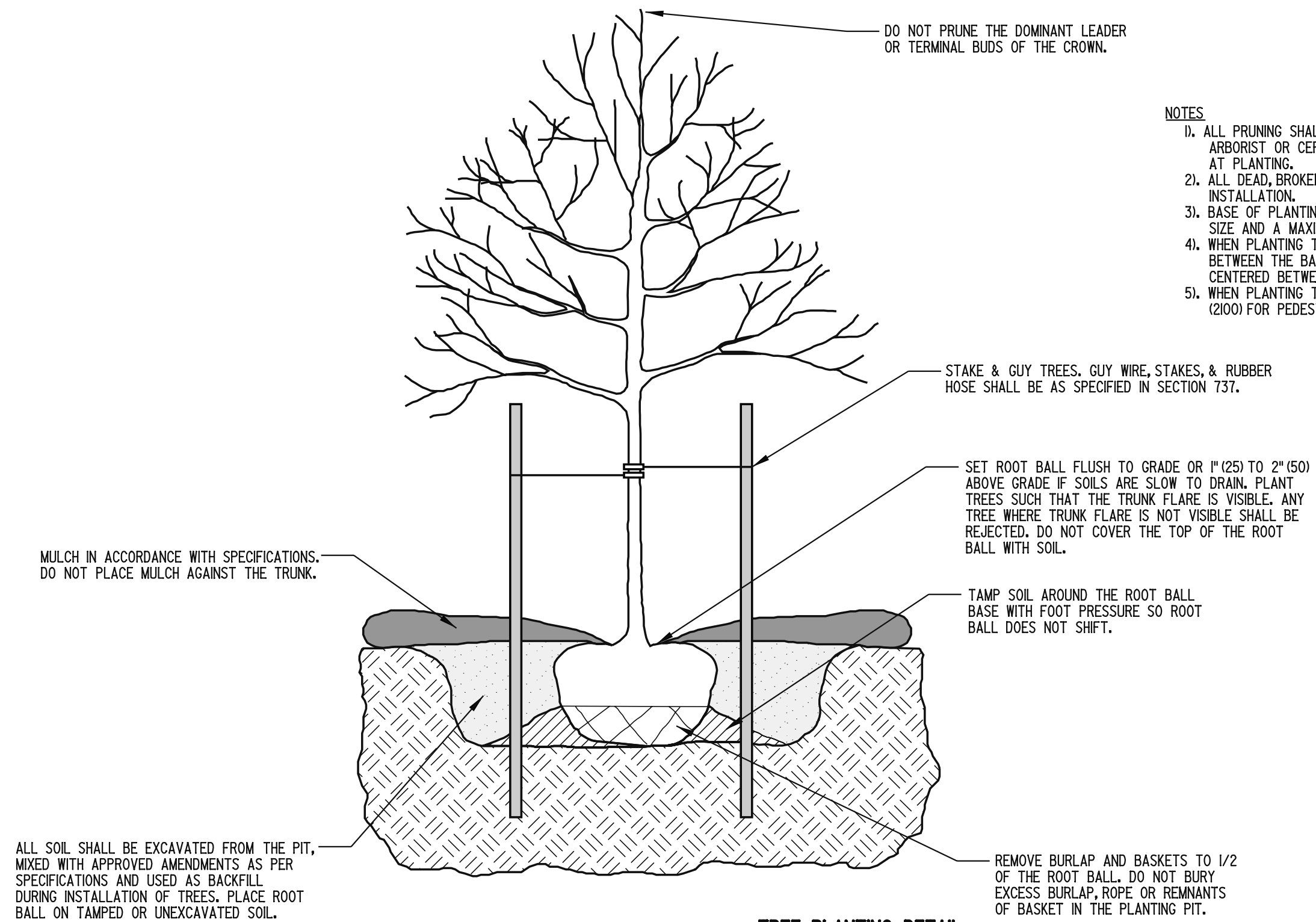


- NOTES:
- 1). BASE OF PLANTING PIT SHALL BE A MINIMUM WIDTH OF TWICE THE ROOT BALL SIZE AND A MAXIMUM OF THREE TIMES THE ROOT BALL SIZE.
 - 2). SHRUBS SHALL BE INSTALLED IN MASSES OF NO LESS THAN 3 PLANTS. A MINIMUM OF 6' (1800) WIDTH IS REQUIRED FROM THE BACK OF CURB TO THE EDGE OF SIDEWALK FOR INSTALLATION OF SHRUBS.
 - 3). ALL PRUNING SHALL BE DONE BY AN I.S.A. CERTIFIED ARBORIST, CERTIFIED NURSERY PROFESSIONAL, OR UNDER THE DIRECTION THEREOF. DO NOT HEAVILY PRUNE SHRUBS AT PLANTING.
 - 4). AUGERED HOLES SHALL BE HAND DUG TO FINAL WIDTH AND TO ELIMINATE GLAZING.
 - 5). ALL SHRUB MASSES SHALL BE MULCHED AS ONE CONTINUOUS BED.



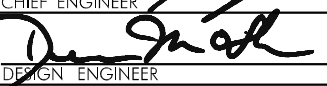
ROADSIDE SHRUB PLANTING DETAIL

 DELAWARE DEPARTMENT OF TRANSPORTATION	PLANTING DETAILS			APPROVED  10/10/06 <small>CHIEF ENGINEER DATE</small>
	STANDARD NO. L-1 (2006)	SHT. 1	OF 3	RECOMMENDED  10/13/06 <small>DESIGN ENGINEER DATE</small>

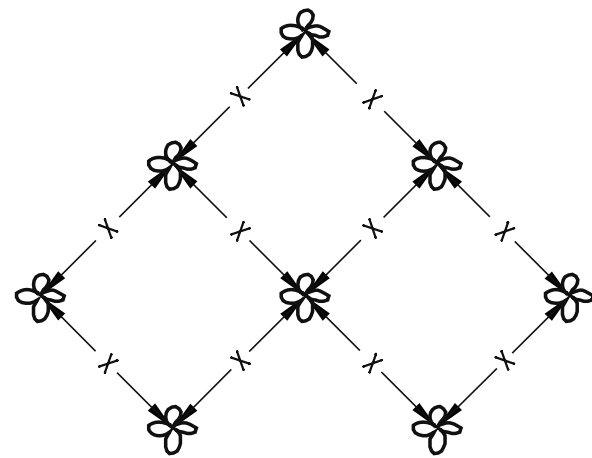


- NOTES**
- 1). ALL PRUNING SHALL BE DONE BY OR UNDER THE DIRECTION OF, AN I.S.A. CERTIFIED ARBORIST OR CERTIFIED NURSERY PROFESSIONAL. DO NOT HEAVILY PRUNE TREES AT PLANTING.
 - 2). ALL DEAD, BROKEN, & CROSSING BRANCHES SHALL BE PRUNED OFF FOLLOWING INSTALLATION.
 - 3). BASE OF PLANTING PIT SIZE SHALL BE A MINIMUM WIDTH OF TWICE THE ROOT BALL SIZE AND A MAXIMUM OF THREE TIMES THE ROOT BALL SIZE.
 - 4). WHEN PLANTING TREES ALONG STREETS, THERE MUST BE A MINIMUM OF 6' (1800) BETWEEN THE BACK OF CURB AND THE EDGE OF SIDEWALK AND SHALL BE CENTERED BETWEEN THE BACK OF CURB AND THE EDGE OF SIDEWALK.
 - 5). WHEN PLANTING TREES ALONG SIDEWALKS, THE TREE SHALL BE LIMBED TO 7' (2100) FOR PEDESTRIAN CLEARANCE.

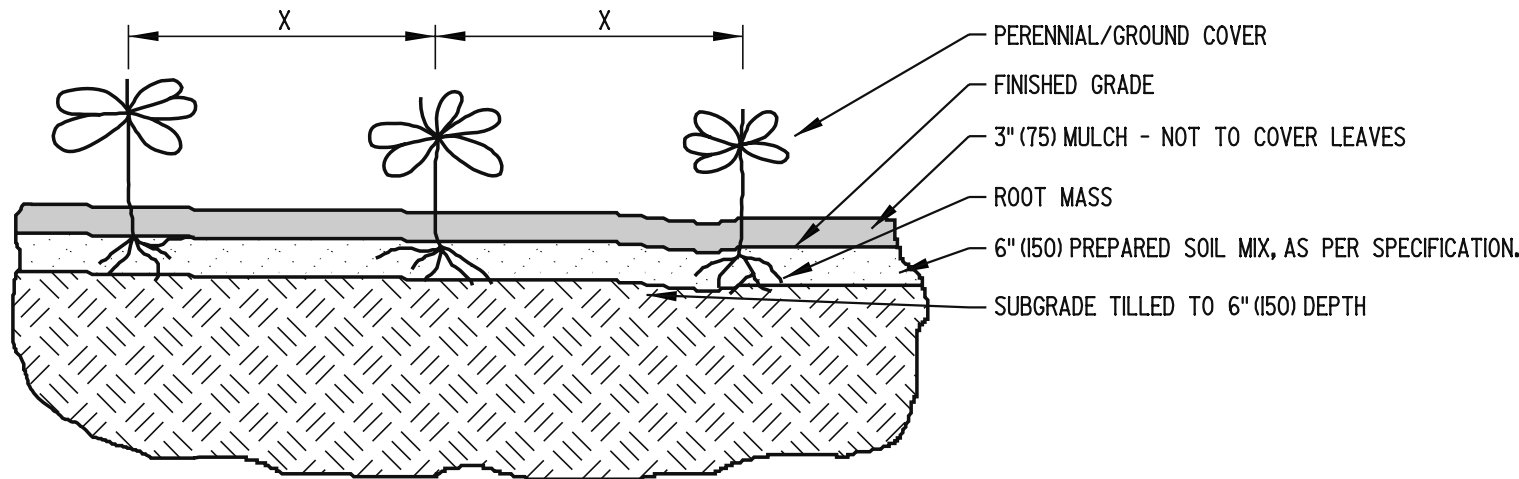
TREE PLANTING DETAIL

 DELAWARE DEPARTMENT OF TRANSPORTATION	PLANTING DETAILS			APPROVED  10/10/06 <small>CHIEF ENGINEER DATE</small>
	STANDARD NO. L-1 (2006)	SHT. 2	OF 3	RECOMMENDED  10/13/06 <small>DESIGN ENGINEER DATE</small>

NOTE:
1). SEE PLANT LIST FOR SPACING (X).






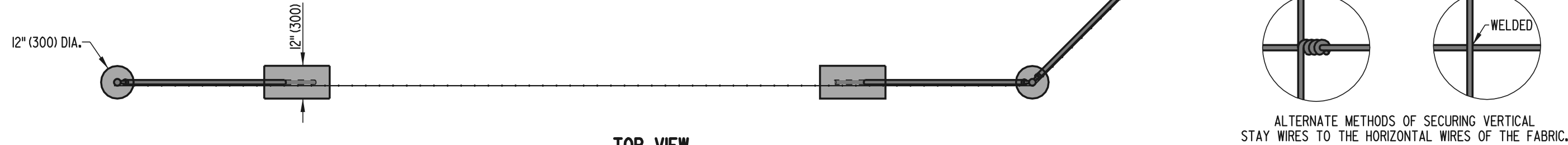
PLAN VIEW



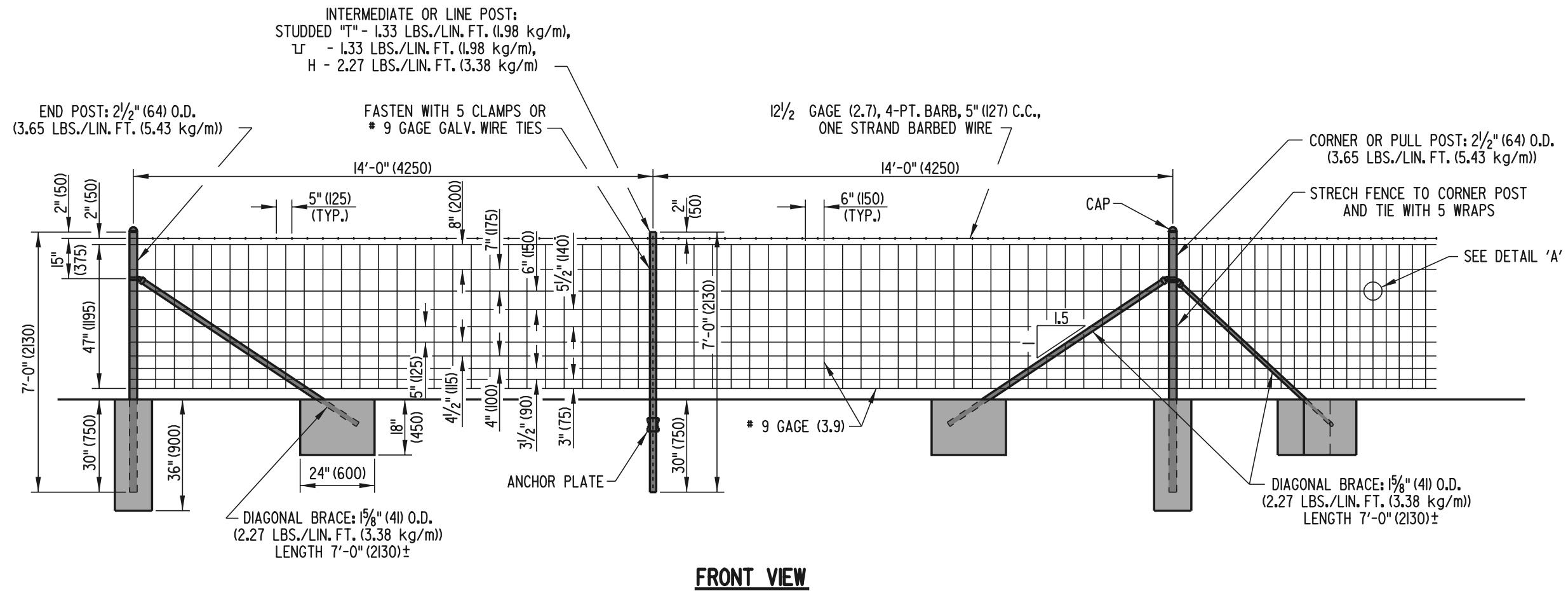
SECTION VIEW



PERENNIAL/GROUNDCOVER PLANTING DETAIL

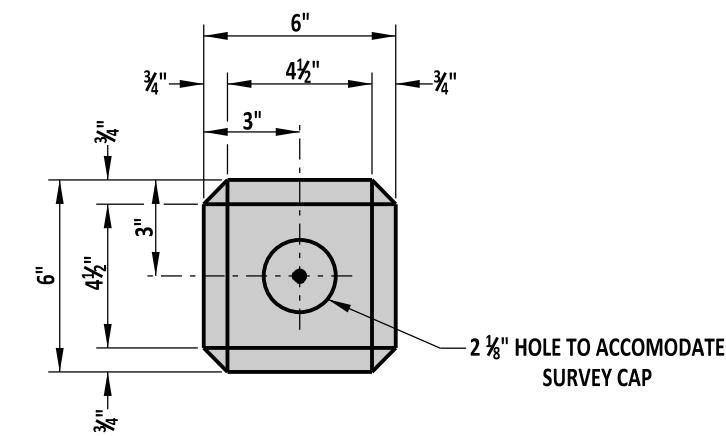
 DELAWARE DEPARTMENT OF TRANSPORTATION	PLANTING DETAILS			APPROVED  <u>10/10/06</u> CHIEF ENGINEER DATE
	STANDARD NO. L-1 (2006)	SHT. 3	OF 3	RECOMMENDED  <u>10/13/06</u> DESIGN ENGINEER DATE



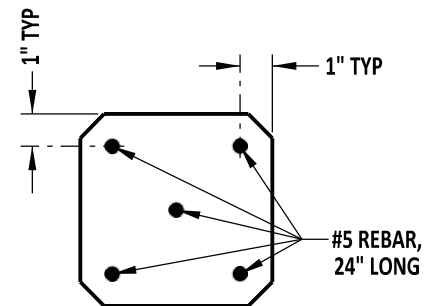
DEATAIL 'A'



 <div>DELAWARE DEPARTMENT OF TRANSPORTATION</div>	RIGHT-OF-WAY FENCE				APPROVED  CHIEF ENGINEER 6/18/01 DATE
	STANDARD NO.	M-1 (2001)	SHT.	1	OF 1



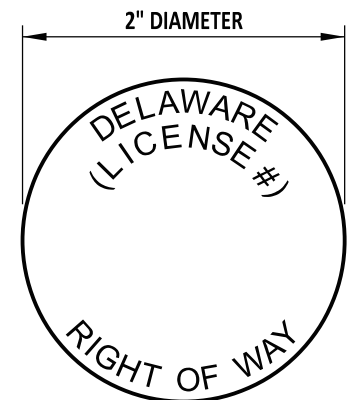
TOP



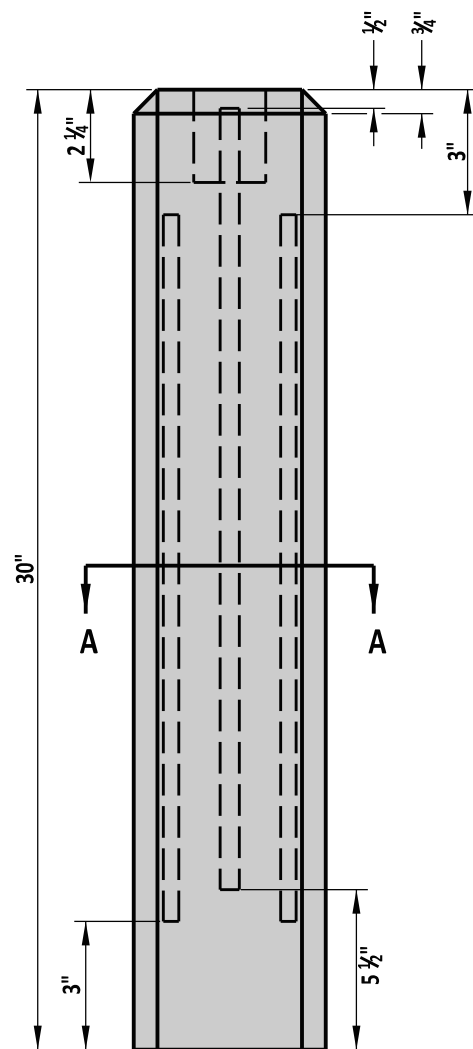
SECTION A-A

- NOTES : 1). LONGITUDINAL STEEL SHALL BE HELD IN PLACE BY CRADLES.
- 2). LETTERS ON CONCRETE MONUMENT TO BE COUNTERSUNK IN TOP OF MARKER 1/4".
- 3). FLEXIBLE DELINEATORS ARE ONLY TO BE USED ON ROADS WITH A SPECIFIED DENIAL OF ACCESS OR CLASSIFIED AS MINOR ARTERIALS OR HIGHER. ON ALL OTHER ROAD CLASSIFICATIONS, A WOODEN STAKE SHALL BE PLACED WITH "ROW" HANDWRITTEN VERTICALLY IN 1" TALL LETTERS.
- 4). PLACE CAP ON CONCRETE MONUMENT SO THAT TOP OF CAP IS FLUSH WITH THE TOP OF THE CONCRETE MONUMENT.

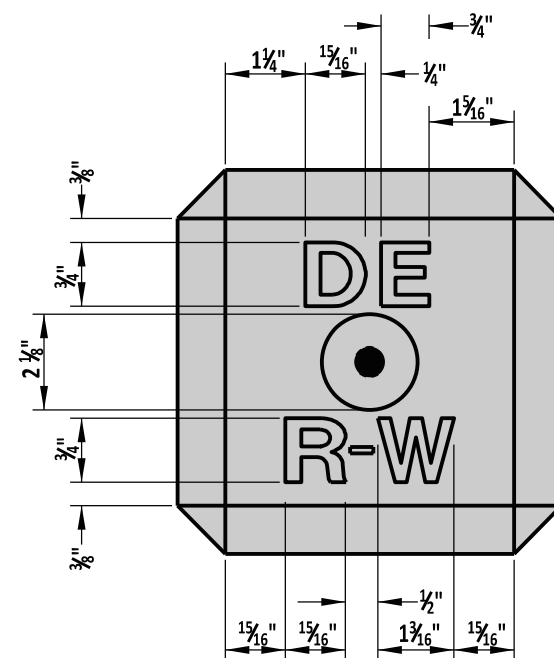
SCALE : NTS



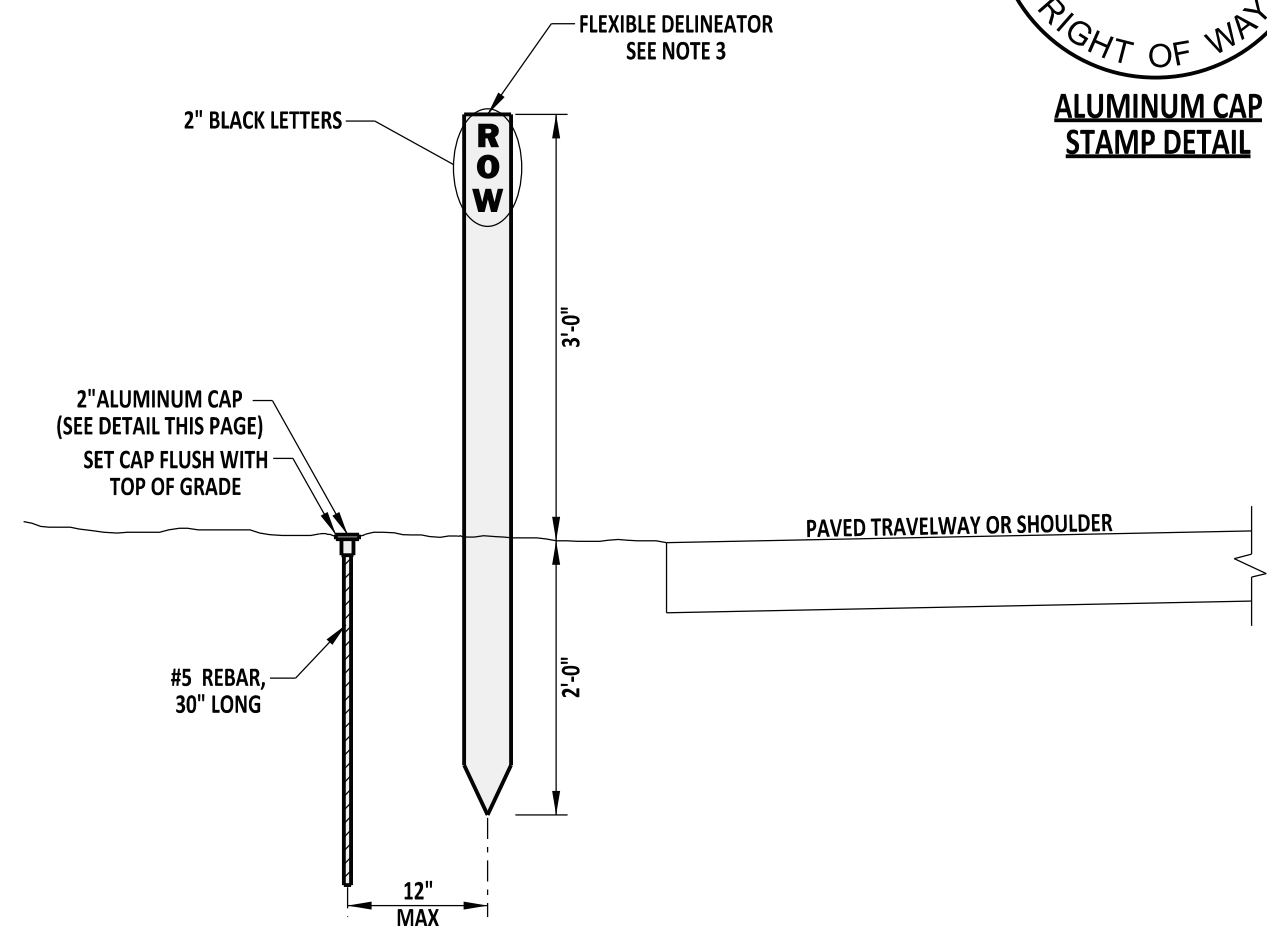
ALUMINUM CAP
STAMP DETAIL



ELEVATION



TOP DETAIL



REBAR AND CAP WITH
FLEXIBLE DELINEATOR DETAIL



DELAWARE
DEPARTMENT OF TRANSPORTATION

RIGHT OF WAY MONUMENTATION

STANDARD NO.

M-2 (2011)

SHT. 1

OF 1

APPROVED

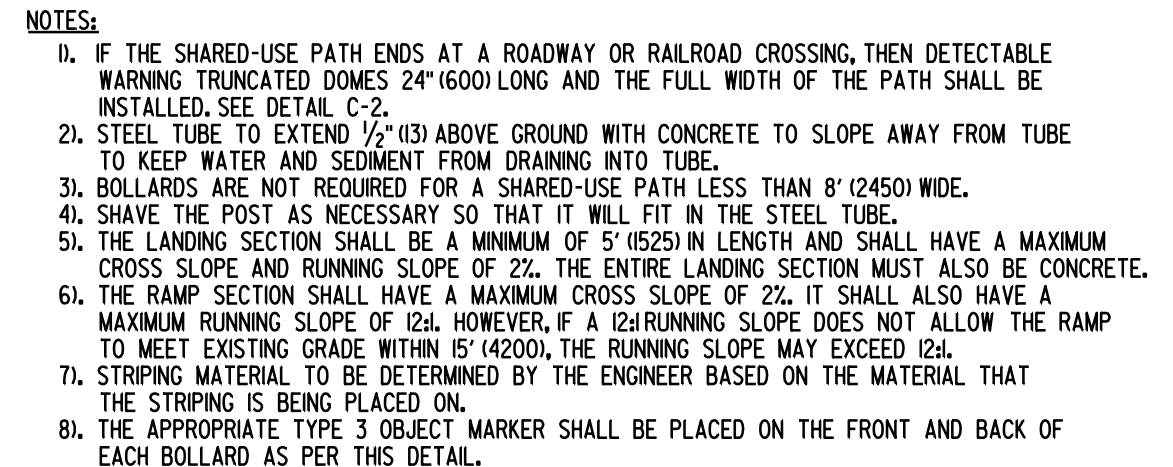
SIGNATURE ON FILE
CHIEF ENGINEER

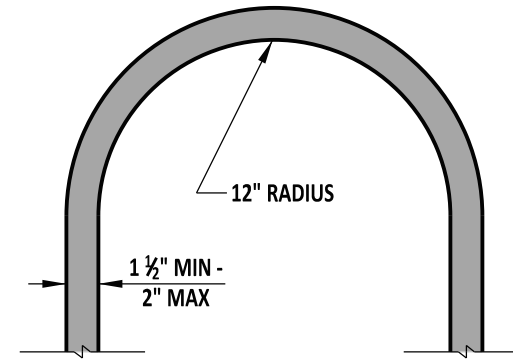
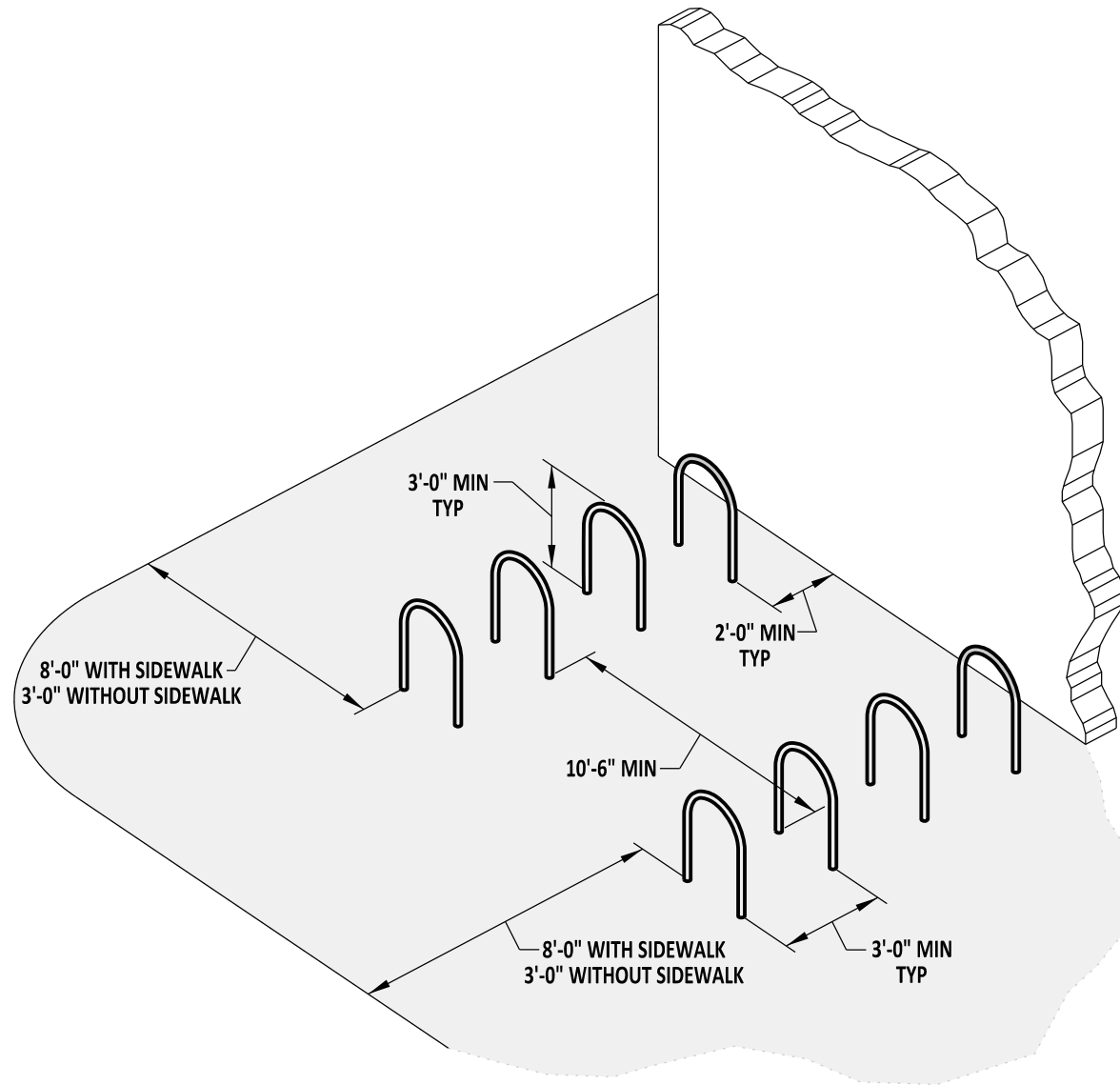
12/22/2011
DATE

RECOMMENDED

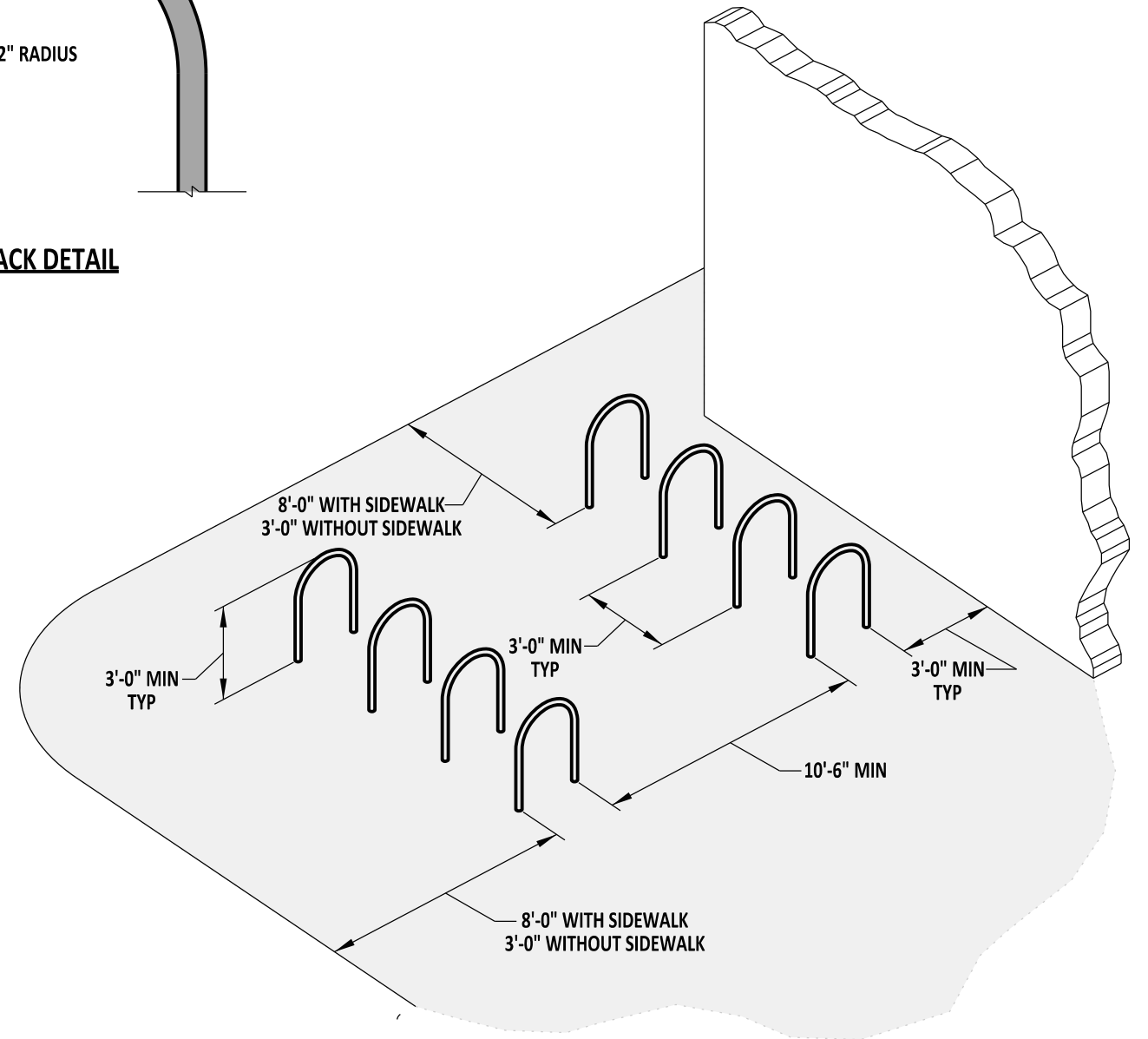
SIGNATURE ON FILE
DESIGN ENGINEER

12/21/2011
DATE





BIKE RACK DETAIL



NOTES:

- 1). BIKE RACK SHALL BE ANCHORED AS PER MANUFACTURER'S RECOMMENDATIONS AFTER APPROVAL FROM ENGINEER IN THE FIELD.
- 2). DETAIL SHOWN WITH P.C.C. CURB TYPE 1-8, HOWEVER ACTUAL CURB VARIES AND SHOULD BE PLACED AS SHOWN ON PLANS.
- 3). SPECIAL CONSIDERATIONS SHOULD BE TAKEN WHEN PLACING BIKE RACKS NEAR CURB RAMPS AND MAY REQUIRE A DETAIL ON THE PLANS.



DELAWARE
DEPARTMENT OF TRANSPORTATION

BIKE RACK LAYOUT DETAILS

STANDARD NO.

M-4 (2011)

SHT. 1

OF 1

APPROVED

SIGNATURE ON FILE
CHIEF ENGINEER

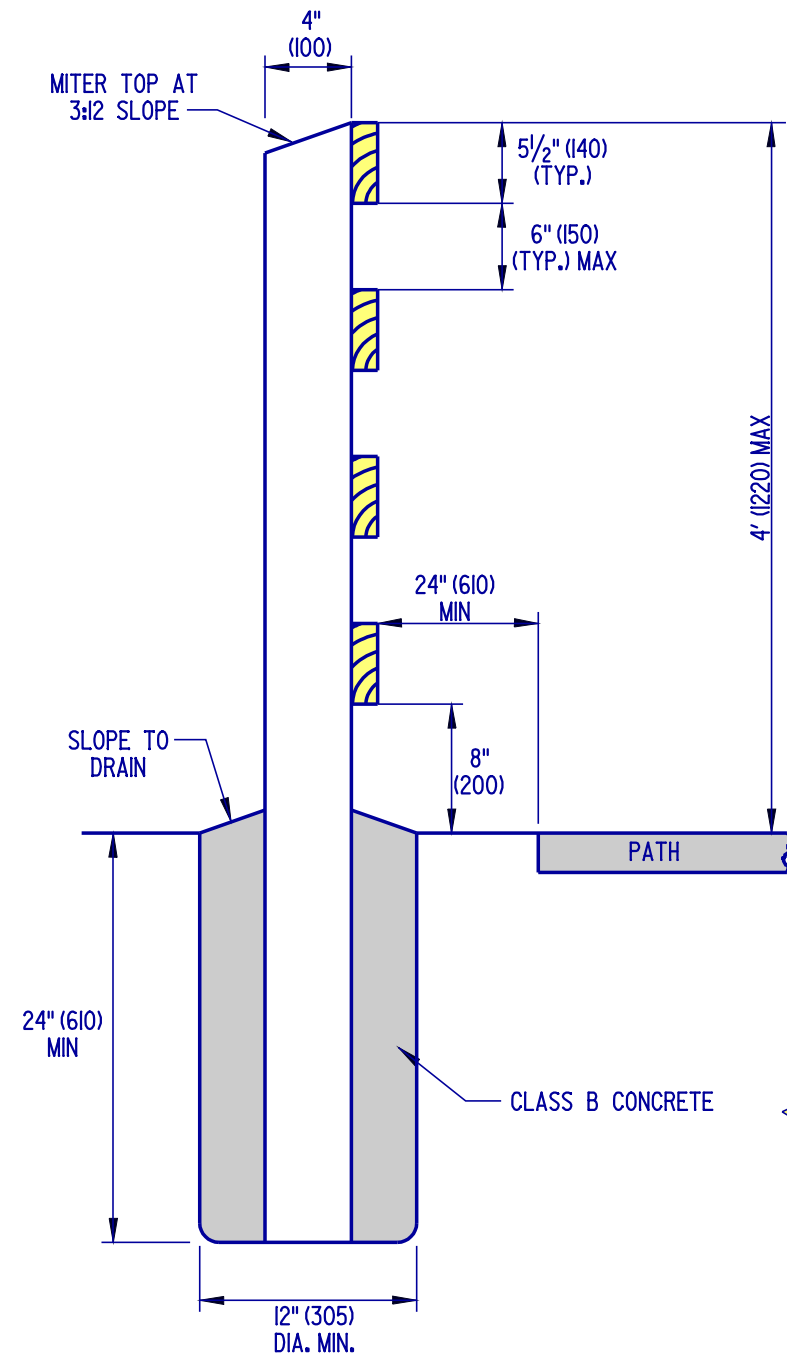
12/22/2011
DATE

RECOMMENDED

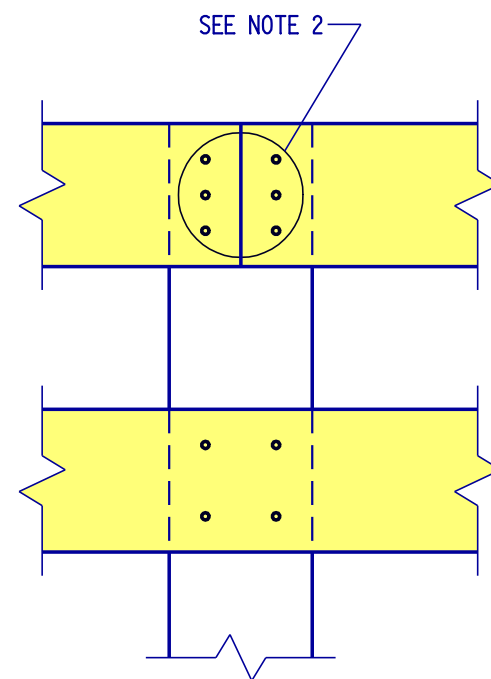
SIGNATURE ON FILE
DESIGN ENGINEER

12/21/2011
DATE

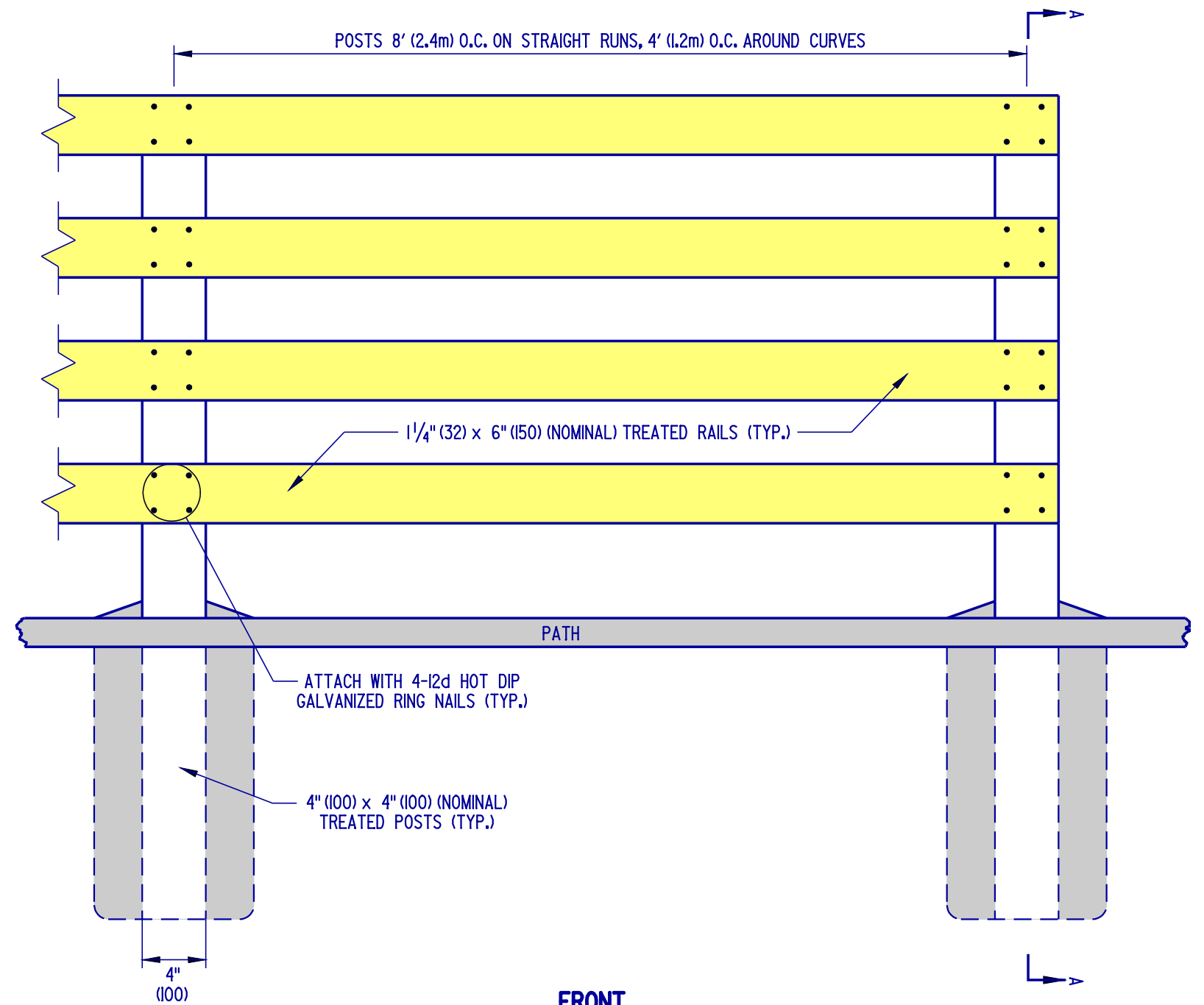
SCALE : N.T.S.



SECTION A-A



TYPICAL JOINT DETAIL



FRONT

NOTES:

1. ALL RAIL JOINTS SHALL BE CENTERED AT THE POSTS.
2. ALL JOINTS SHALL BE ATTACHED WITH 3 - 12d NAILS AND TWO ADJACENT RAILS SHALL NOT END ON THE SAME POST.
3. RAILS SHALL BE FLUSH TO THE POSTS AT THE END POSTS.



**DELAWARE
DEPARTMENT OF TRANSPORTATION**

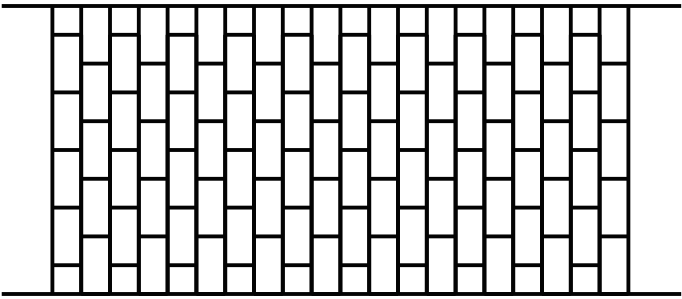
WOOD RAIL FENCE DETAILS

STANDARD NO. M-5 (2004)

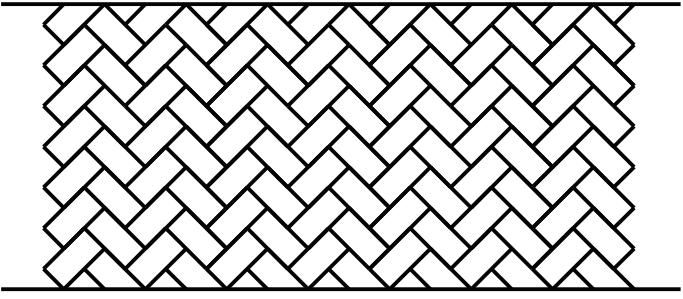
SHT. 1 OF 1

APPROVED Carolann Wicks 1/10/05
CHIEF ENGINEER DATE

CHIEF ENGINEER	DATE
RECOMMENDED <i>Dennis M. O'Flaherty</i>	<i>1/3/05</i>
DESIGN ENGINEER	DATE

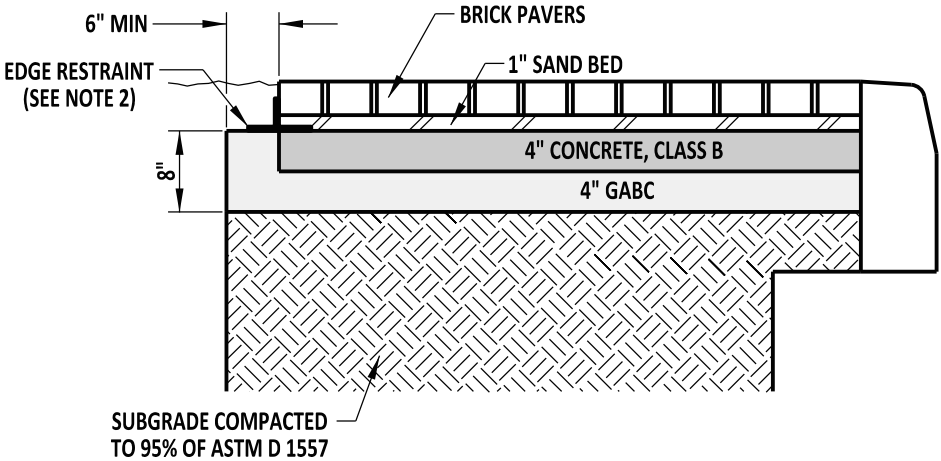


4" x 8" RUNNING BOND PATTERN



4" x 8" HERRINGBONE PATTERN

- NOTES:
- 1. ACTUAL PATTERN TO BE USED SHALL BE SPECIFIED ON THE PLANS. COLOR IS TO BE "BRICK RED" UNLESS OTHERWISE NOTED ON THE PLANS.
 - 2. MATERIALS AND PAVEMENT BOX VARY DEPENDING ON PLANS.
 - 3. FOR CROSSWALK APPLICATIONS, REFER TO THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STRIPING WIDTH.
 - 4. THE PATTERNS ABOVE ARE THE PREFERRED PATTERNS AVAILABLE FOR SIDEWALK OR CROSSWALK APPLICATIONS.



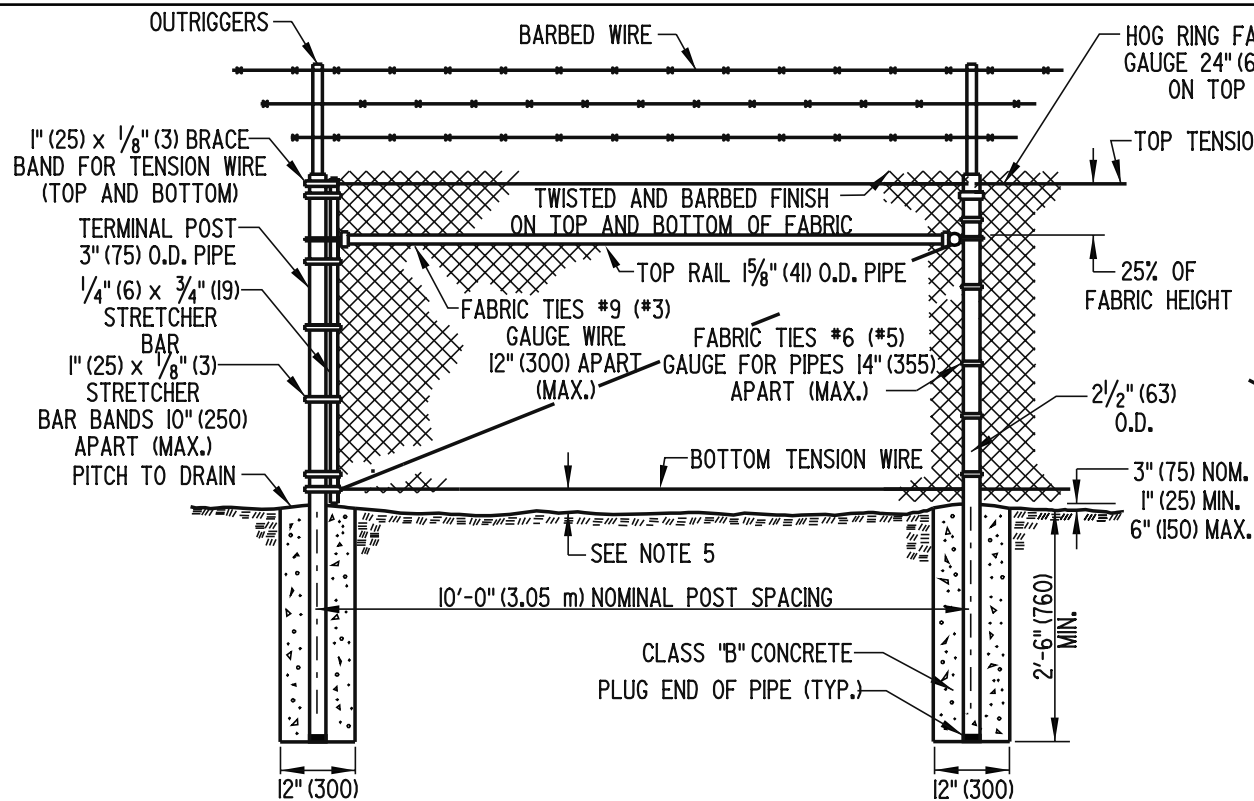
BRICK PAVER SIDEWALK DETAIL

- NOTES:
- 1. WHEN SIDEWALK IS CONFINED BY A RIGID STRUCTURE ON BOTH SIDES, EXPANSION JOINT MATERIAL SHALL BE USED FROM TOP OF BRICK TO BOTTOM OF CONCRETE BASE ON AT LEAST ONE SIDE OF THE SIDEWALK.
 - 2. EDGE RESTRAINT MUST BE APPROVED BY THE ENGINEER IN THE FIELD AND SHALL BE INSTALLED AS PER MANUFACTURER'S RECOMMENDATIONS.



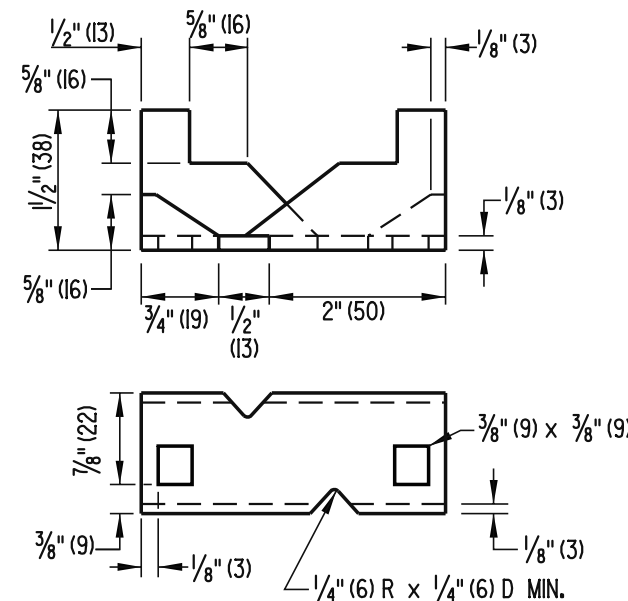
DELAWARE
DEPARTMENT OF TRANSPORTATION

PATTERNED HOT-MIX OR CONCRETE & BRICK PAVER DETAILS				APPROVED	SIGNATURE ON FILE	01/17/2012
					CHIEF ENGINEER	DATE
STANDARD NO.	M-6 (2011)	SHT.	1	OF	1	RECOMMENDED
					SIGNATURE ON FILE	01/17/2012
					DESIGN ENGINEER	DATE

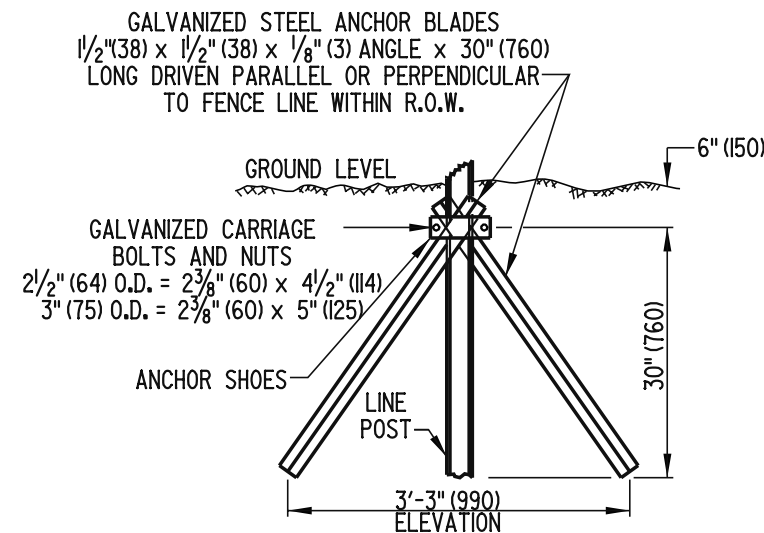


CHAIN-LINK FENCE

TENSION WIRE CONNECTION AT ROUND INTERMEDIATE OR CORNER POST

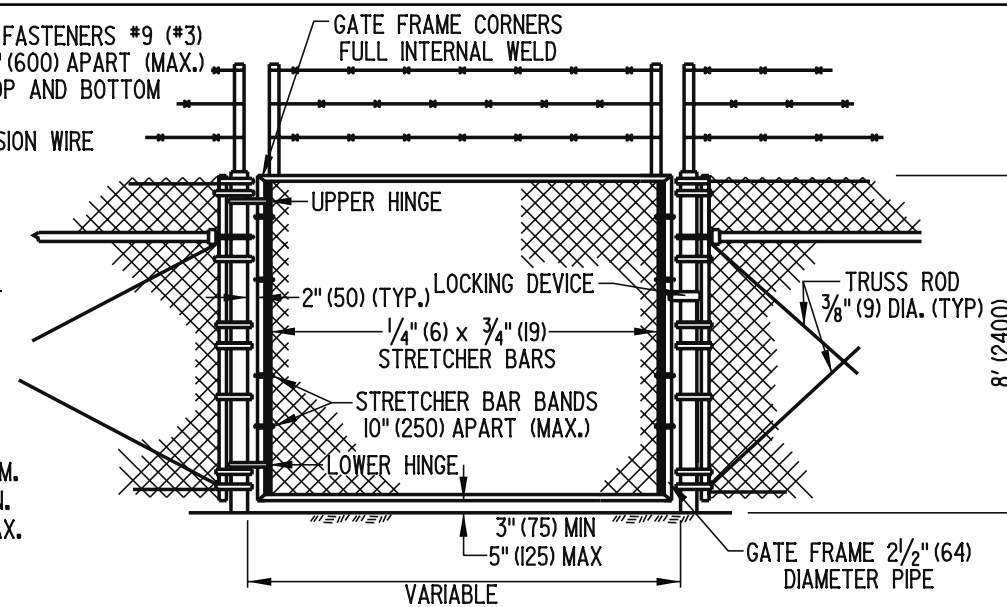


ANCHOR SHOE

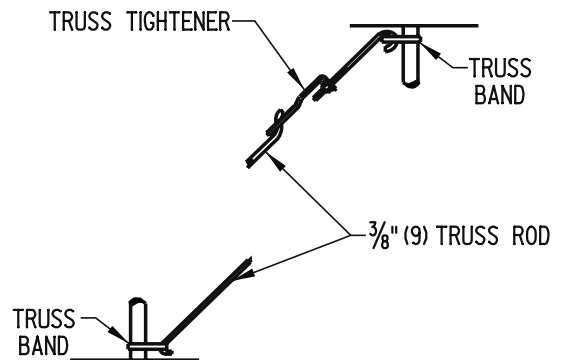


DRIVE ANCHOR SHOE ASSEMBLY

(SEE NOTE 4)



GATES, CHAIN-LINK FENCE



3/8" (9) TRUSS ROD ASSEMBLY

CHAIN-LINK FENCE ASSEMBLIES

GENERAL NOTES

1). POSTS

	TERMINAL, CORNER AND GATE POSTS	LINE POSTS	TOP OR BRACE RAIL
	3" (75) O.D. PIPE	2 1/2" (64) O.D. PIPE	1 5/8" (41) O.D. PIPE
AASHTO TYPE	1 OR II	1 OR II	1 OR II
AASHTO GRADE	1 OR 2	1 OR 2	1 OR 2
MINIMUM LENGTH OF POST:	10'-8" (3250)	10'-8" (3250)	N/A
ACTUAL OUTSIDE DIAMETER	2 7/8" (73)	2 3/8" (60)	1.660" (42)
WALL THICKNESS	GRADE 1 = .203" (5.2) GRADE 2 = .160" (4)	GRADE 1 = .154" (3.9) GRADE 2 = .120" (3)	GRADE 1 = .140" (3.5) GRADE 2 = .111" (2.8)

- THE DEPTH OF CONCRETE FOOTERS IN SOLID ROCK MAY BE REDUCED TO 12" (300) BELOW THE TOP OF ROCK AND THE DIAMETER OF THE HOLE IN ROCK MAY BE REDUCED TO 6" (150).
- BRACE BANDS AND STRETCHER BAR BANDS SHALL BE FURNISHED WITH 3/16" (8) DIA. CARRIAGE BOLTS AND ELASTIC STOP NUTS.
- DRIVE ANCHOR SHOE ASSEMBLY ONLY TO BE USED IN WET AREAS AND WITH PRIOR APPROVAL OF THE ENGINEER.
- THE BOTTOM OF THE FENCE SHALL BE 2" (50) MAX ABOVE HARD GROUND OR PAVEMENT. WHERE THERE IS SOFT GROUND, THE BOTTOM OF THE FENCE SHALL EXTEND INTO THE GROUND IN ORDER TO BE FIRM DUE TO SHIFTING SOIL OR SAND.
- NUTS AND BOLTS SHALL BE TACK WELDED OR BURRED TO PREVENT REMOVAL.
- IF THERE ARE ANY OPENINGS IN THE FENCE LARGER THAN 96 SQ. IN. (620 sq. cm) DUE TO UTILITIES OR GRADED TERRAIN, THE OPENINGS SHALL BE SECURED WITH A METAL GRILL THAT IS LOCKED OR PERMANENTLY WELDED.
- VEGETATION AND PERMANENT STRUCTURES (SUCH AS BUILDINGS, LIGHT POLES, AND UTILITY POLES) SHALL BE AT LEAST 14' (4.2 m) FROM THE FENCE. ANY EXCEPTIONS SHALL REQUIRE THE CONSTRUCTION OF TOP GUARDS.



**DELAWARE
DEPARTMENT OF TRANSPORTATION**

CHAIN LINK FENCE DETAILS

STANDARD NO. M-7 (2006)

SHT. 1 OF 1

APPROVED

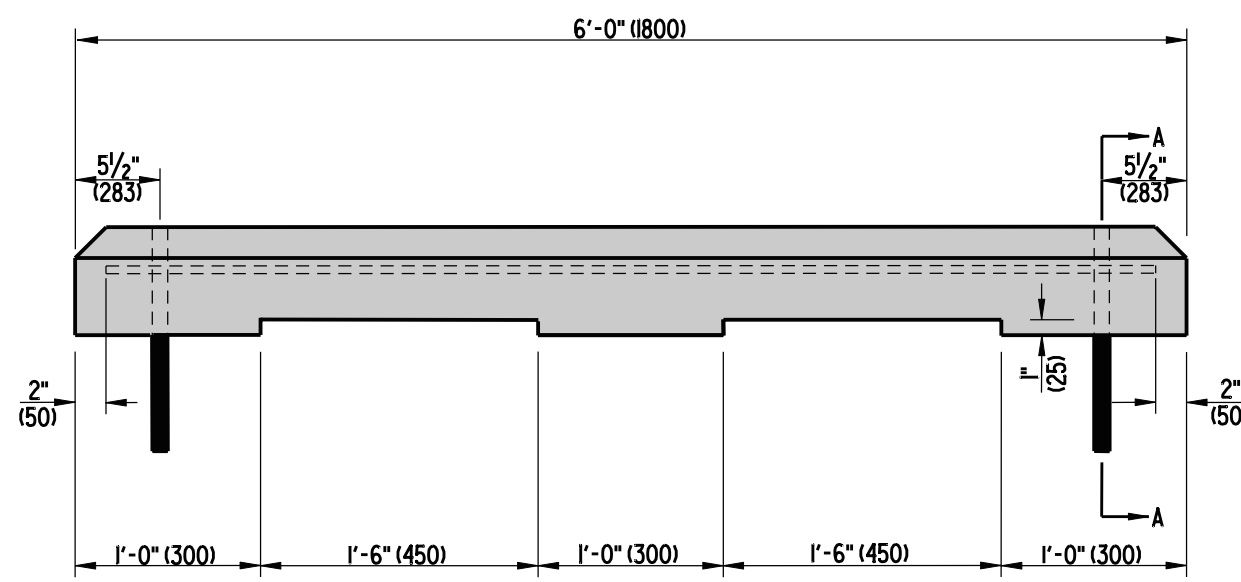
Frank Taylor
CHIEF ENGINEER

10/10/06
DATE

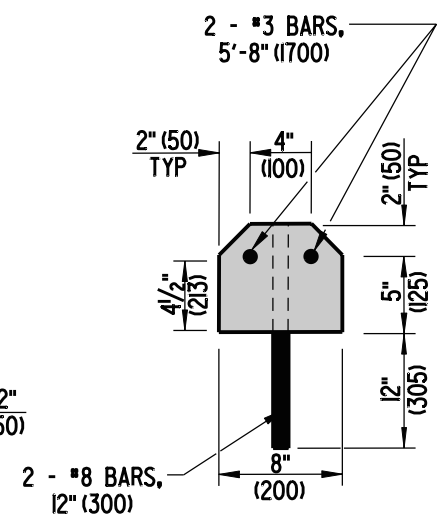
RECOMMENDED

Dan Smith
DESIGN ENGINEER




10/13/06
DATE



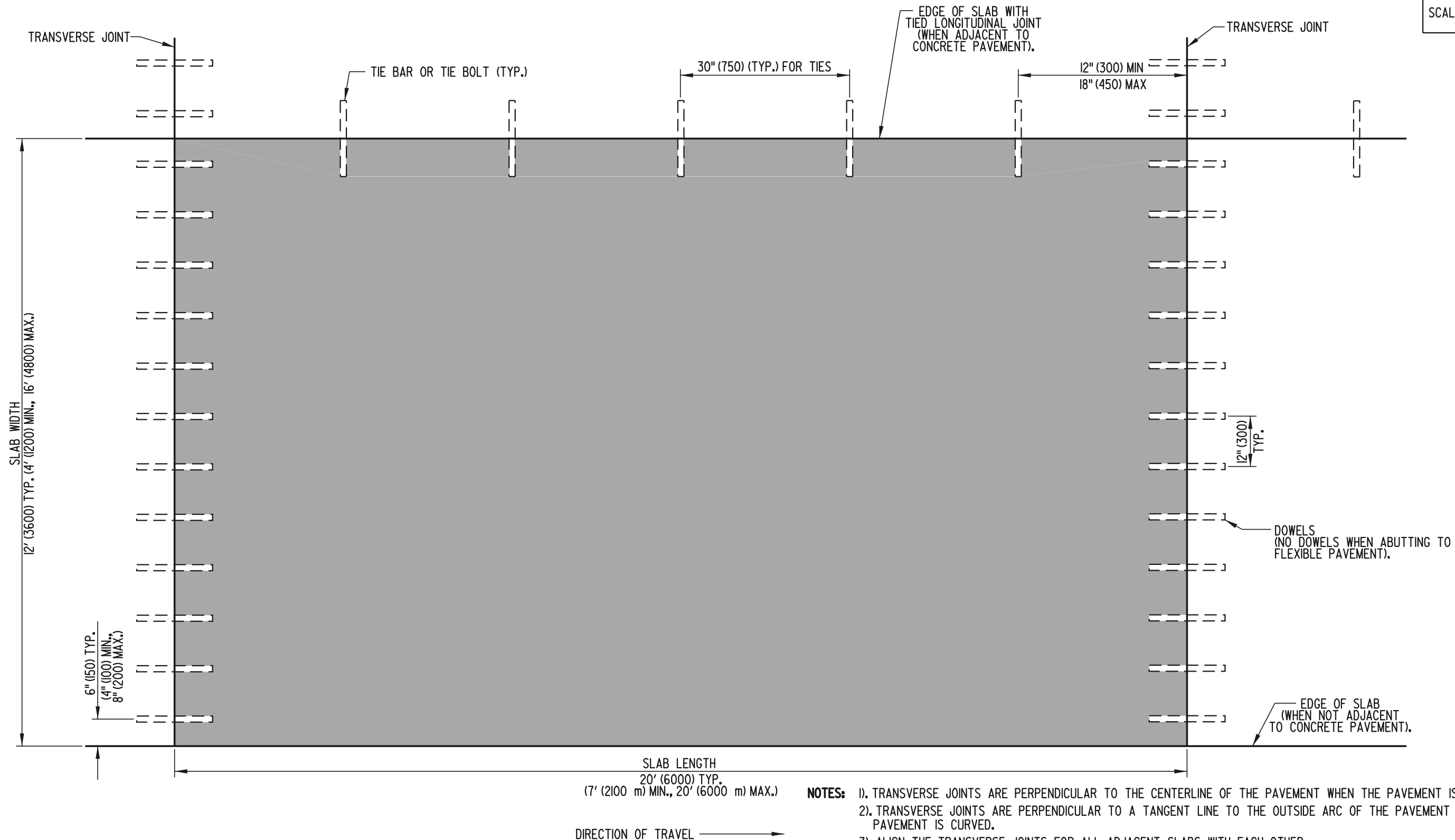
ELEVATION



SECTION A-A

 DELAWARE DEPARTMENT OF TRANSPORTATION	P.C.C. PARKING BUMPER			APPROVED  10/24/07 CHIEF ENGINEER DATE
	STANDARD NO. M-8 (2007)	SHT. 1	OF 1	RECOMMENDED  10/23/07 DESIGN ENGINEER DATE

SCALE : N.T.S.



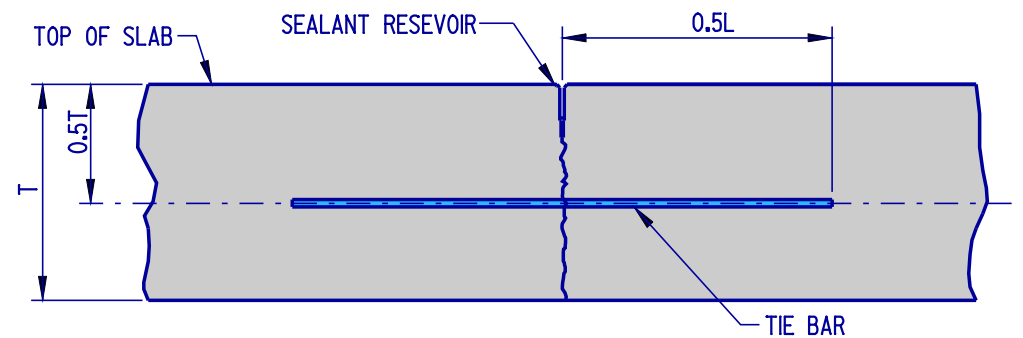
- NOTES:**
- 1). TRANSVERSE JOINTS ARE PERPENDICULAR TO THE CENTERLINE OF THE PAVEMENT WHEN THE PAVEMENT IS STRAIGHT.
 - 2). TRANSVERSE JOINTS ARE PERPENDICULAR TO A TANGENT LINE TO THE OUTSIDE ARC OF THE PAVEMENT WHEN THE PAVEMENT IS CURVED.
 - 3). ALIGN THE TRANSVERSE JOINTS FOR ALL ADJACENT SLABS WITH EACH OTHER.
 - 4). ABRUPT CHANGES IN PAVEMENT WIDTH MAY OCCUR ONLY AT THE TRANSVERSE JOINT LINE; LONGITUDINAL JOINTS SHALL BE CONTINUOUS WHENEVER POSSIBLE.
 - 5). LONGITUDINAL JOINTS SHOULD NOT BE LOCATED WITHIN PROPOSED WHEEL PATHS. THE WHEEL PATH IS GENERALLY LOCATED 2' (600) INSIDE OF THE LANE EDGE LINE OR CENTERLINE.

SLAB PLAN (WITH DOWEL AND TIE LOCATIONS)

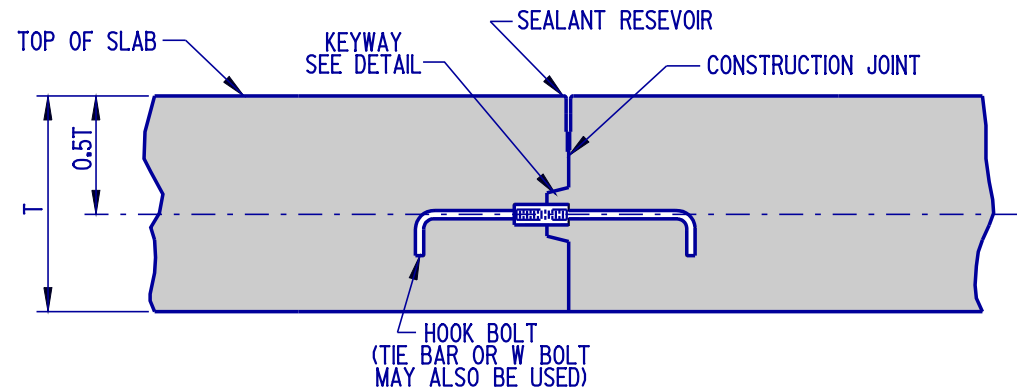


DELAWARE
DEPARTMENT OF TRANSPORTATION

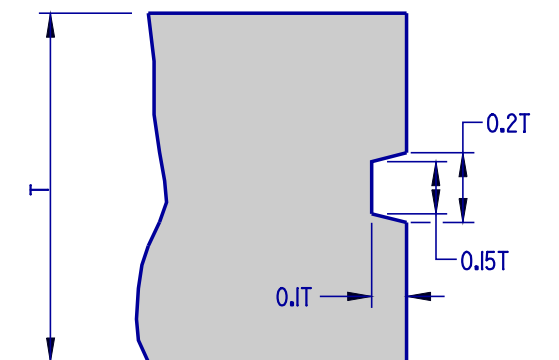
P.C.C. PAVEMENT				APPROVED	6/18/01
STANDARD NO.	P-1 (2001)	SHT.	1	OF	5
				RECOMMENDED	6/18/01



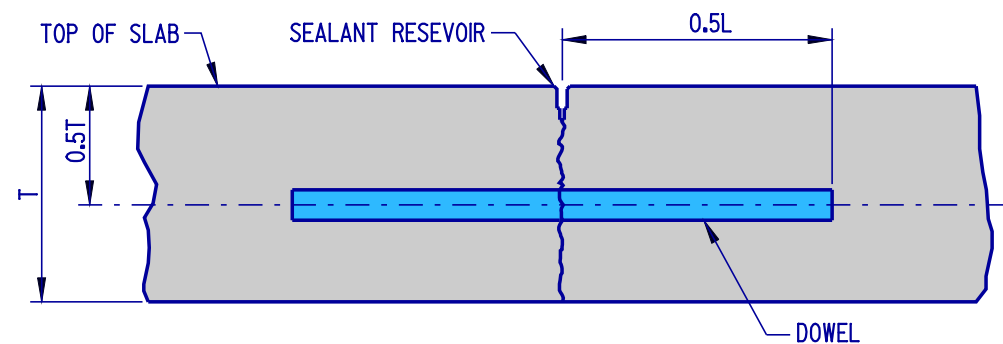
LONGITUDINAL SAW-CUT JOINT DETAIL



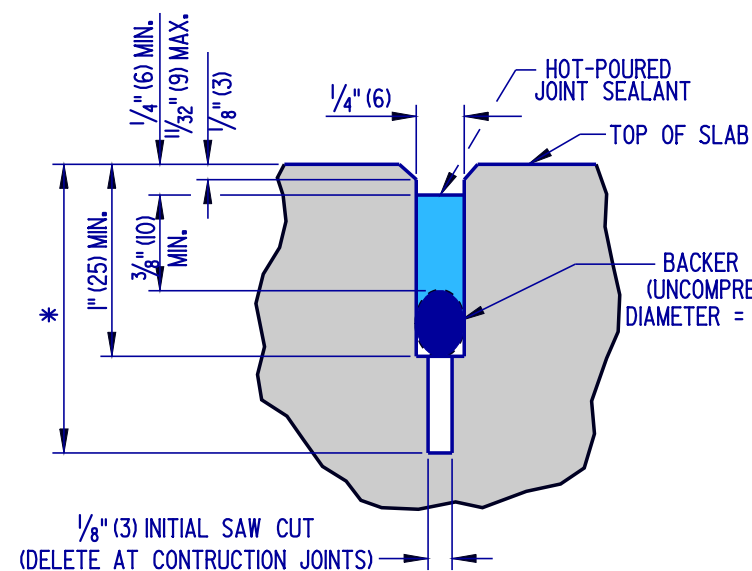
LONGITUDINAL CONSTRUCTION JOINT DETAIL



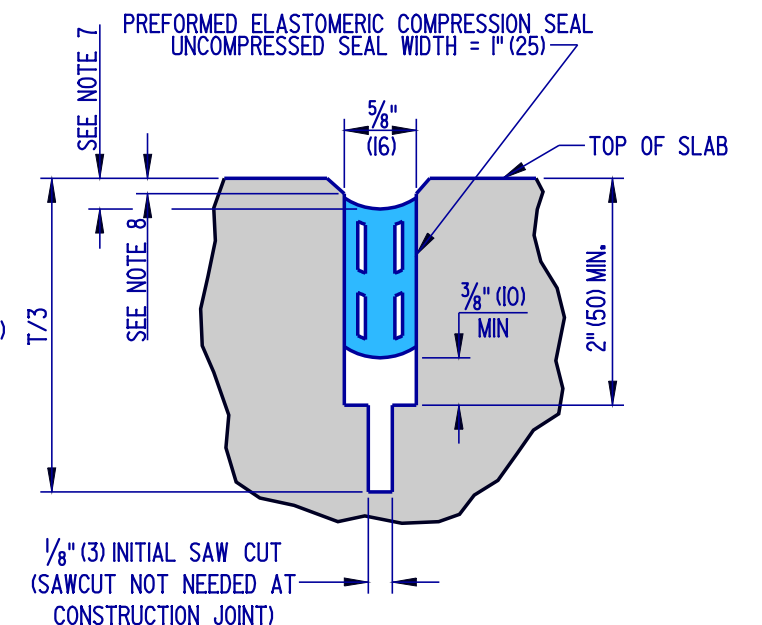
KEYWAY DETAIL



TRANSVERSE SAW-CUT JOINT DETAIL



**SEALANT DETAIL-
LONGITUDINAL JOINT**



**SEALANT DETAIL-
TRANSVERSE JOINT**

* - 0.3T (10\"/>

NOTES:

- 1). AS DIMENSIONED, THE WIDTH OF THE TRANSVERSE SEALANT RESERVOIR IS APPLICABLE WHEN THE TEMPERATURE OF THE PAVEMENT SURFACE IS BETWEEN 60°F (16°C) AND 80°F (27°C). WHEN THE TEMPERATURE IS BELOW 60°F (16°C), THE SEALANT RESERVOIR SHALL BE CUT 1/16\"/>

JOINT AND SEALANT DETAILS



**DELAWARE
DEPARTMENT OF TRANSPORTATION**

STANDARD NO. P-1 (2004)

P.C.C.PAVEMENT

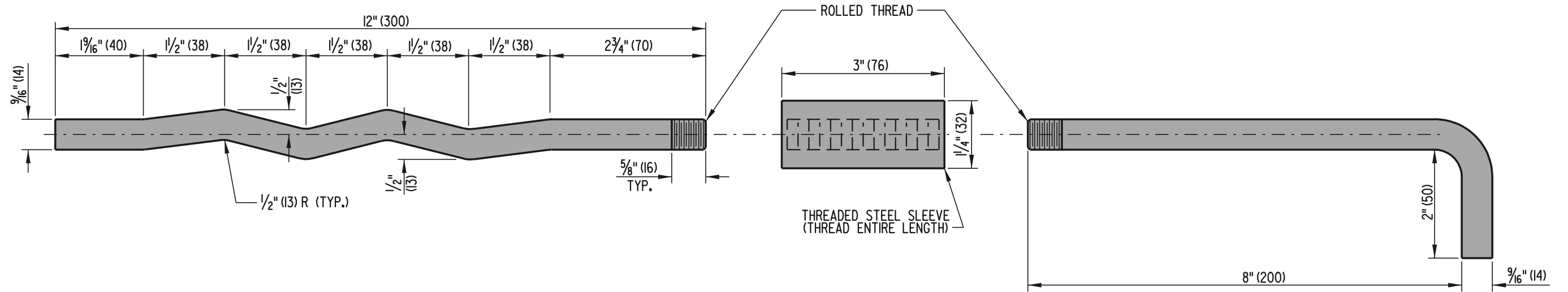
SHT. 2 OF 5

APPROVED

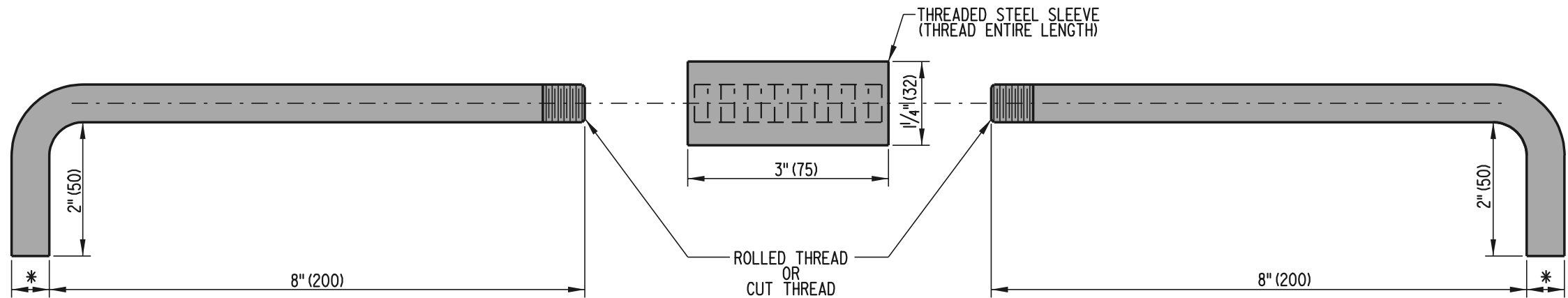
Carolann Wick 1/10/05
CHIEF ENGINEER DATE

RECOMMENDED

Dennis M. O'Flaherty 1/13/05
DESIGN ENGINEER DATE

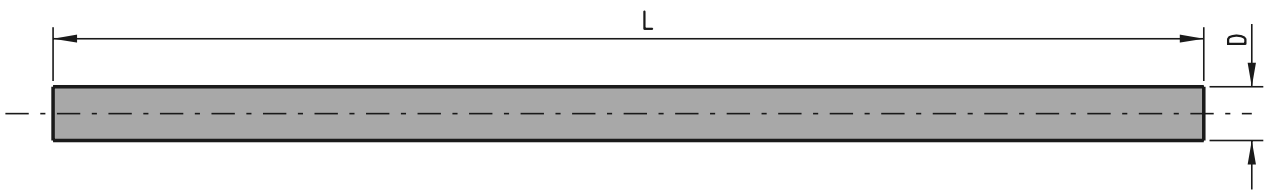


W BOLT



HOOK BOLT

* -1 1/16" (17) ROLLED THREADS
3/4" (19) CUT THREADS



DOWEL & TIE BAR

DOWEL & TIE BAR CHART				
SLAB THICKNESS	DOWEL		TIE BAR	
	D	L	D	L
10" (250)	1 1/4" (32)	18" (450)	5/8" (16)	30" (750)
12" (300)	1 1/2" (38)	20" (500)	5/8" (16)	30" (750)



DELAWARE
DEPARTMENT OF TRANSPORTATION

P.C.C. PAVEMENT

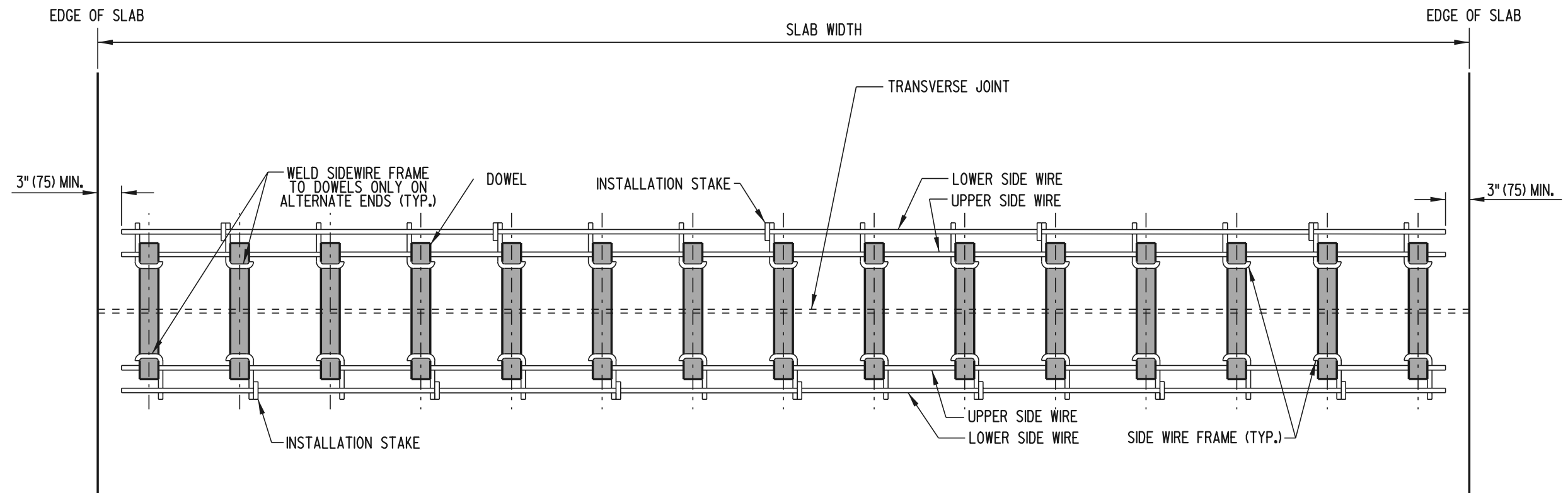
STANDARD NO. P-1 (2001)

SHT. 3 OF 5

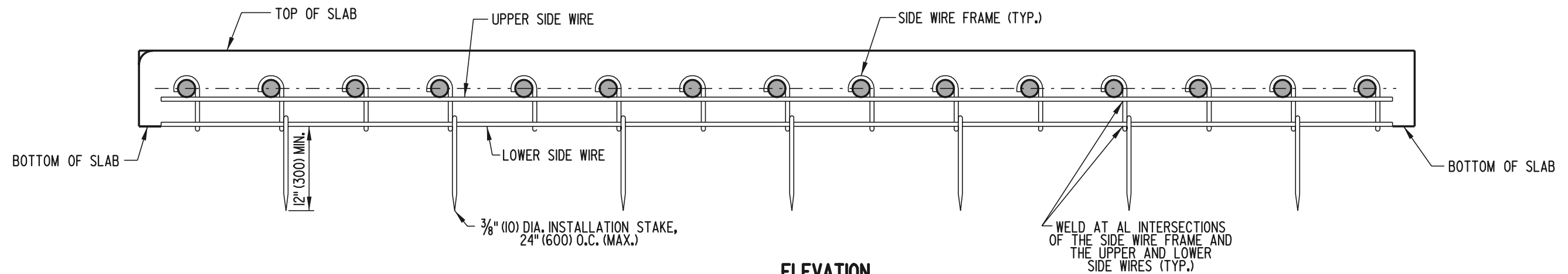
APPROVED *Ryan M. Hershman* 6/18/01
CHIEF ENGINEER DATE

RECOMMENDED *Michael J. Gotsch* 6/18/01
DESIGN ENGINEER DATE

SCALE : N.T.S.



PLAN



ELEVATION

DOWEL SUPPORT BASKET



**DELAWARE
DEPARTMENT OF TRANSPORTATION**

P.C.C. PAVEMENT

STANDARD NO. P-1 (2001)

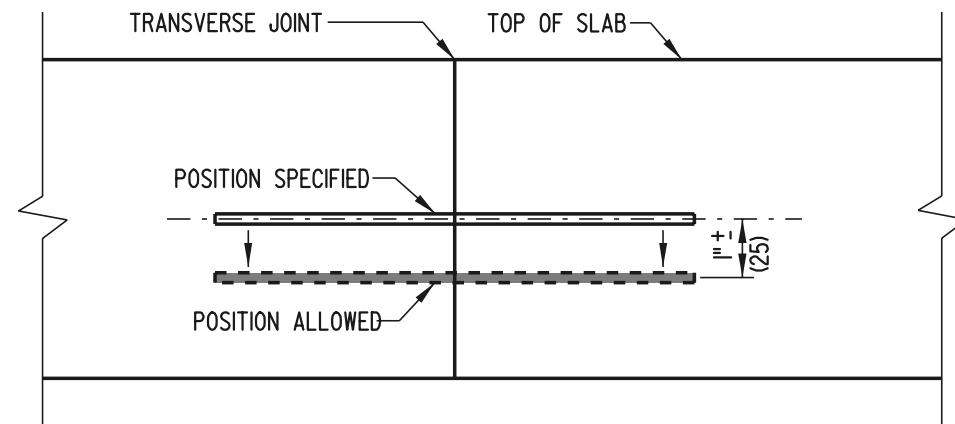
SHT. 4 OF 5

APPROVED

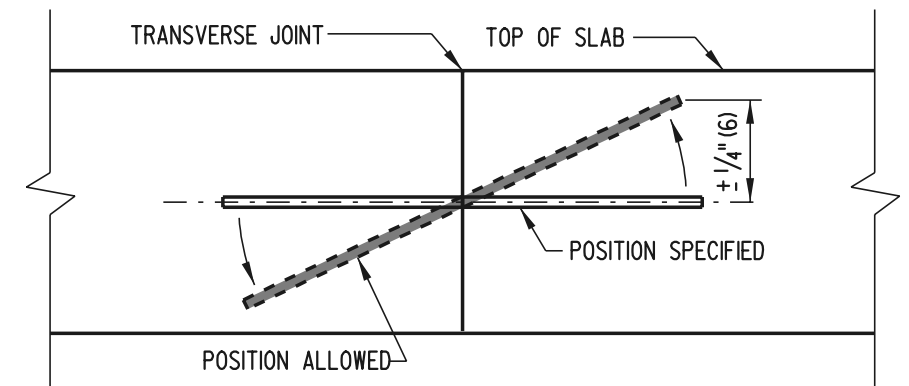
Ryan M. Harkness **6/18/01**
CHIEF ENGINEER DATE

RECOMMENDED

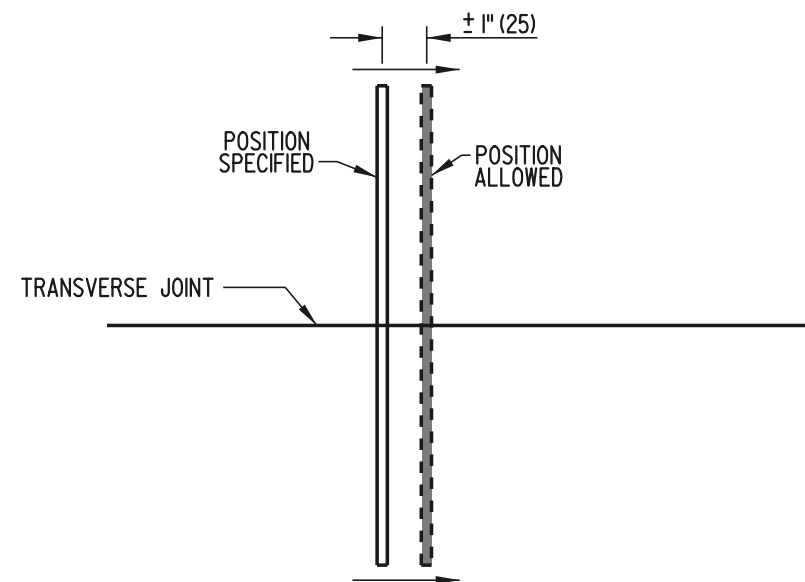
Mehal Alghamdi **6/18/01**
DESIGN ENGINEER DATE



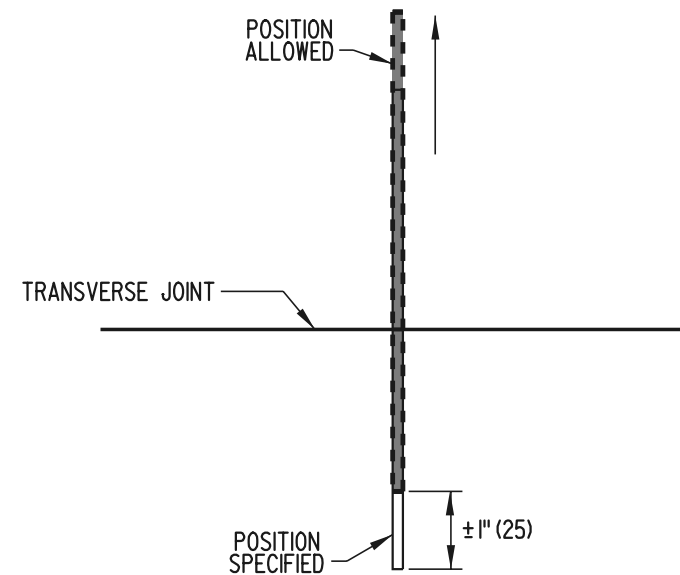
VERTICAL TRANSLATION



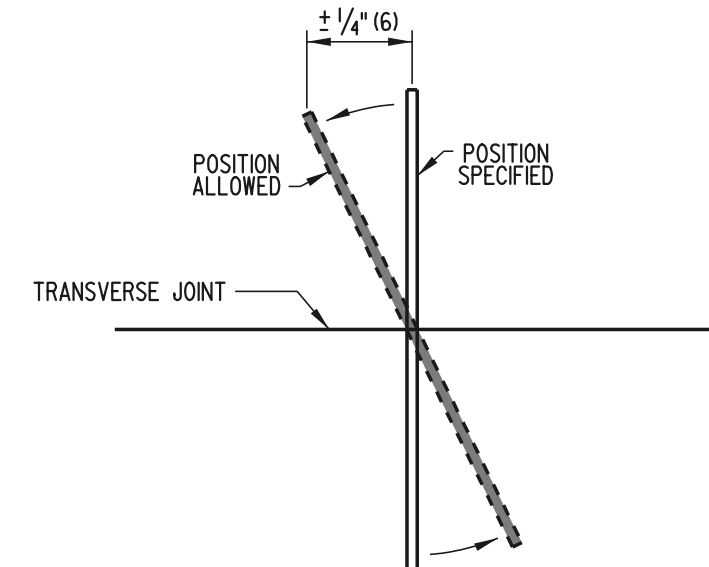
VERTICAL ROTATION



HORIZONTAL TRANSLATION



LONGITUDINAL TRANSLATION



HORIZONTAL ROTATION

DOWEL & TIE BAR PLACEMENT TOLERANCES



**DELAWARE
DEPARTMENT OF TRANSPORTATION**

P.C.C. PAVEMENT

STANDARD NO. P-1 (2001)

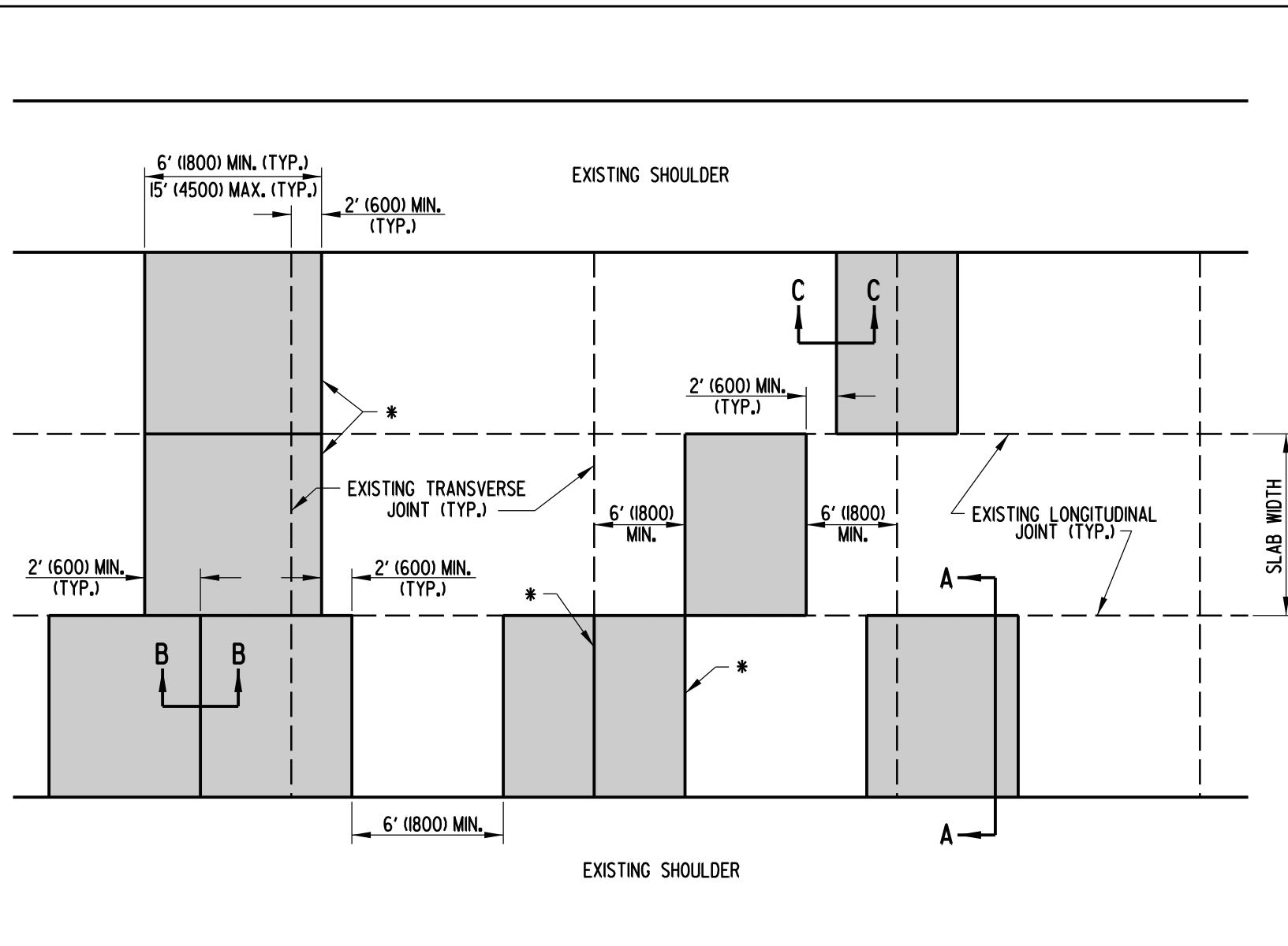
SHT. 5 OF 5

APPROVED

Ryan M. Harkness **6/18/01**
CHIEF ENGINEER DATE

RECOMMENDED

Michael R. Gotsch **6/18/01**
DESIGN ENGINEER DATE



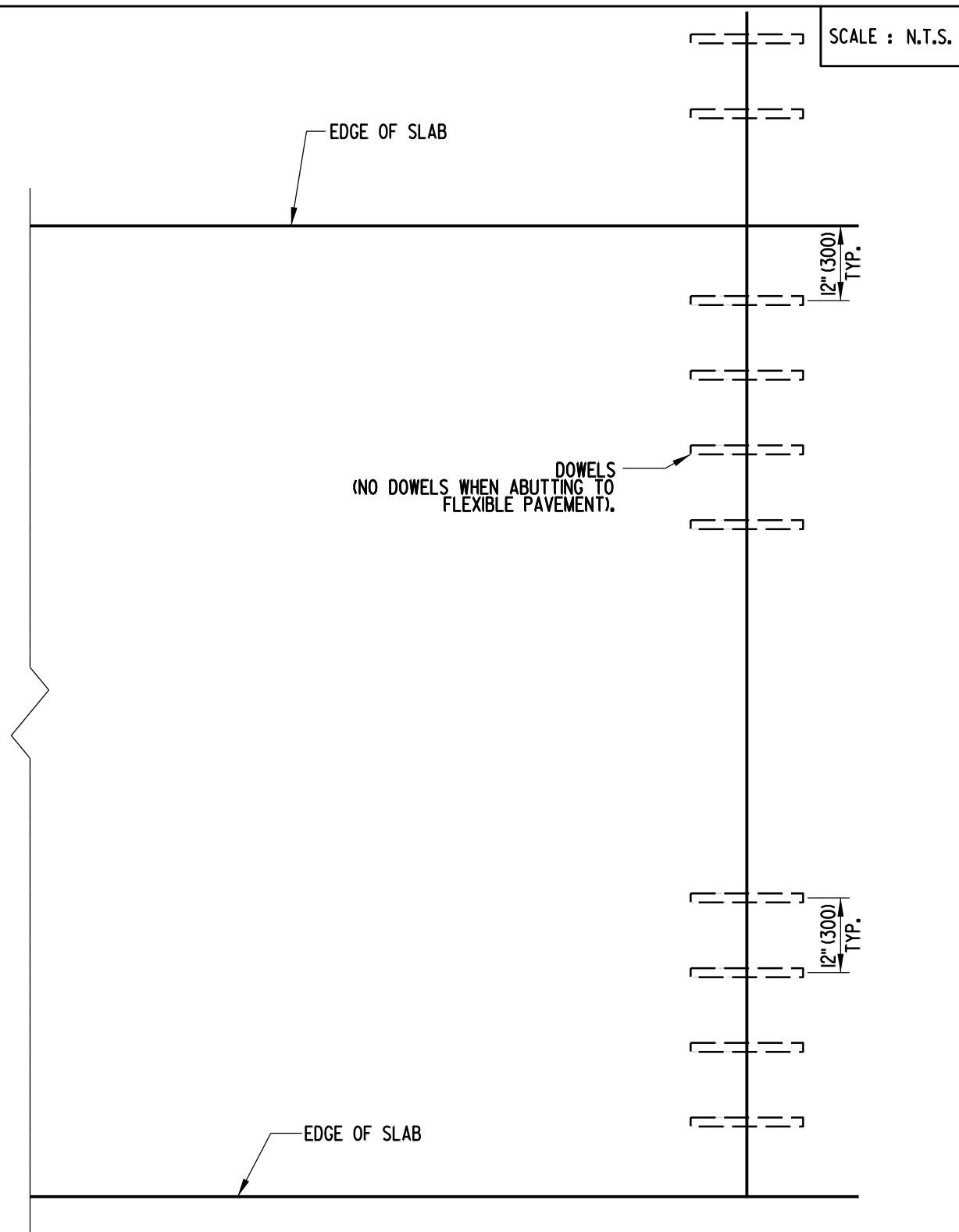
PLAN

* - PROPOSED LOCATIONS FOR TRANSVERSE JOINTS SHALL EXACTLY MATCH THE ALIGNMENT OF THE FINAL (EXISTING OR RELOCATED) TRANSVERSE JOINTS IN ALL IMMEDIATELY ADJACENT LANES.

NOTES:

1. WHEN REPAIRING EXISTING TRANSVERSE JOINTS, THE PATCH SHALL EXTEND A MINIMUM OF 24" (600) THROUGH THE EXISTING JOINT, WHICH WILL RELOCATE THE JOINT.
2. PROPOSED LOCATIONS FOR TRANSVERSE JOINTS, WHEN NOT ALIGNED WITH THE FINAL EXPECTED TRANSVERSE JOINT LOCATIONS IN THE IMMEDIATELY ADJACENT LANES, SHALL BE OFFSET A MINIMUM OF 2' (600) FROM THE AFFORMENTIONED JOINTS.
3. THE LONGITUDINAL JOINT ALIGNMENT SHALL BE STRAIGHT AND CONTINUOUS THROUGH THE REPAIRED AREA.

FULL DEPTH PATCH



SLAB PLAN (WITH DOWEL AND TIE LOCATIONS)



DELAWARE
DEPARTMENT OF TRANSPORTATION

P.C.C. PAVEMENT PATCHING

STANDARD NO. P-2 (2008)

SHT. 1 OF 5

APPROVED

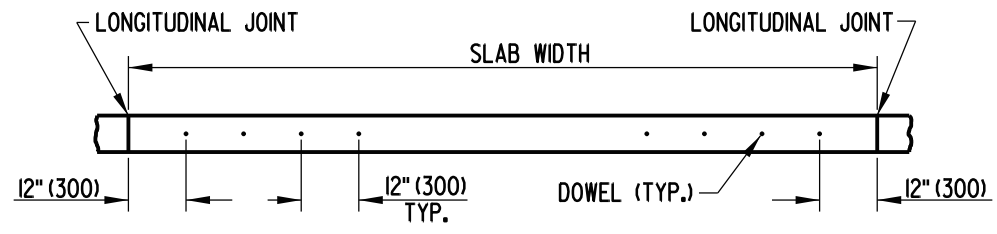
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CHIEF ENGINEER

11/18/08
DATE

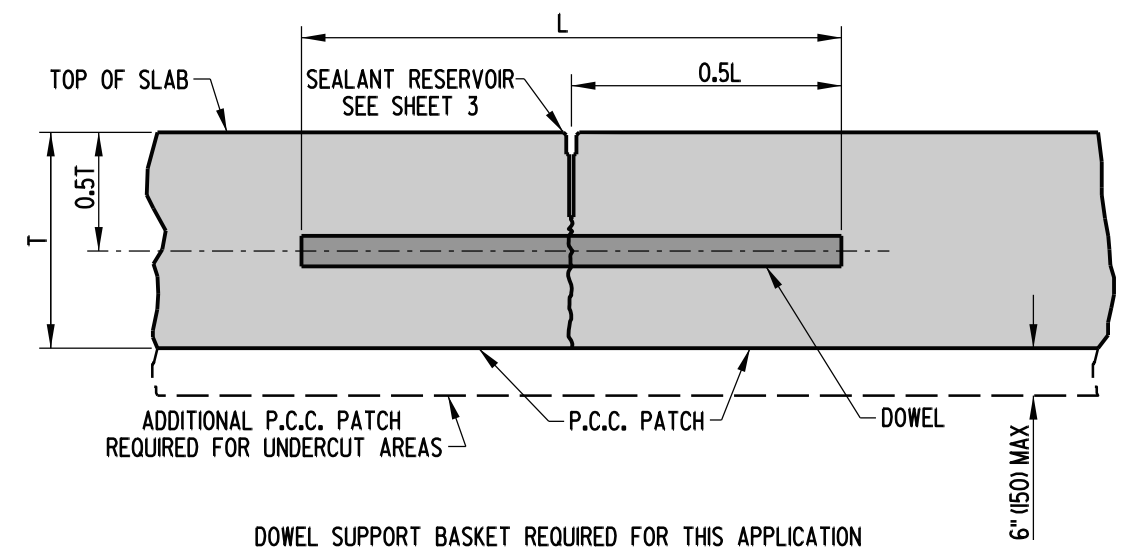
RECOMMENDED

[Signature]
DESIGN ENGINEER

11/17/08
DATE



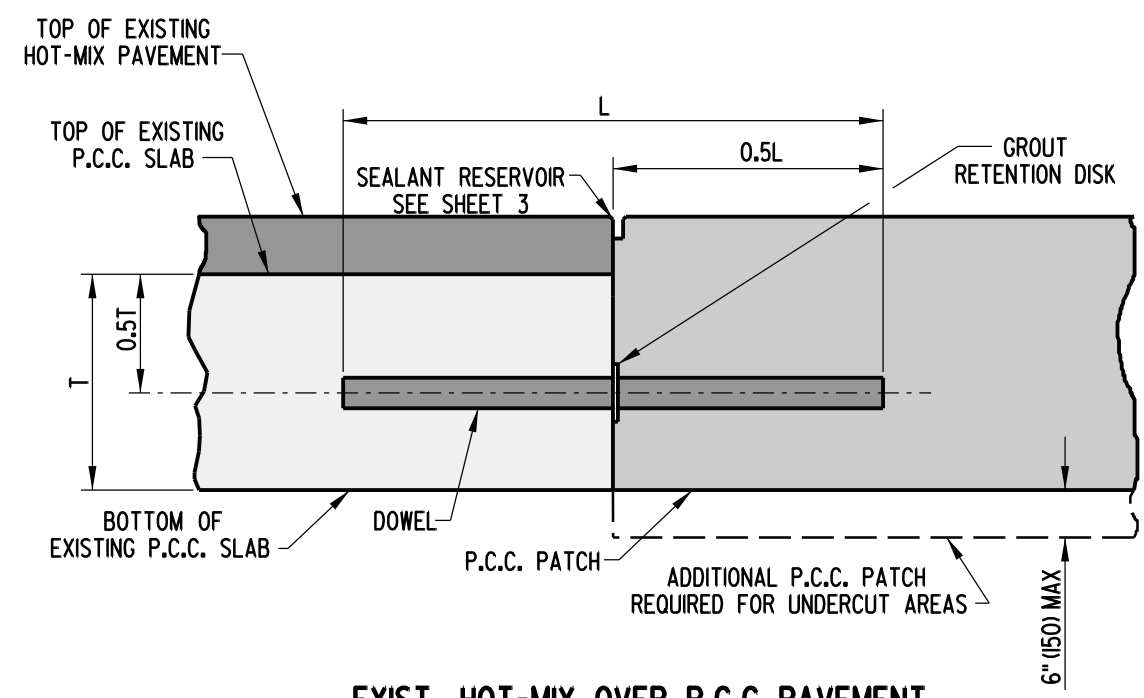
SECTION A-A



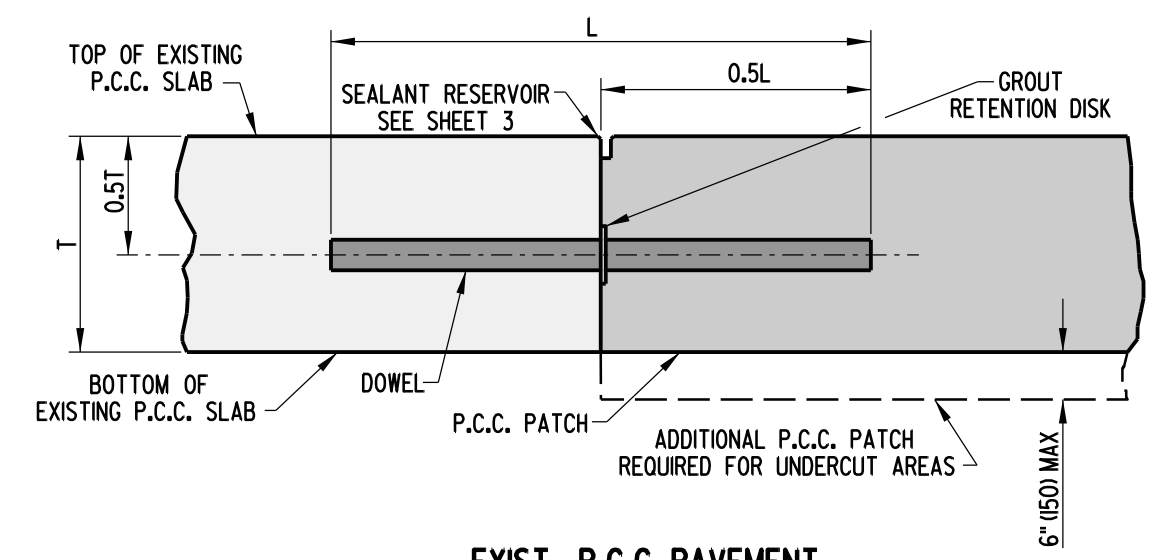
DOWEL SUPPORT BASKET REQUIRED FOR THIS APPLICATION
(REFER TO STANDARD CONSTRUCTION DETAIL FOR P.C.C. PAVEMENT.)

SECTION B-B

TRANSVERSE SAW-CUT USED FOR
JOINTS LOCATED WITHIN THE PATCH



EXIST. HOT-MIX OVER P.C.C. PAVEMENT






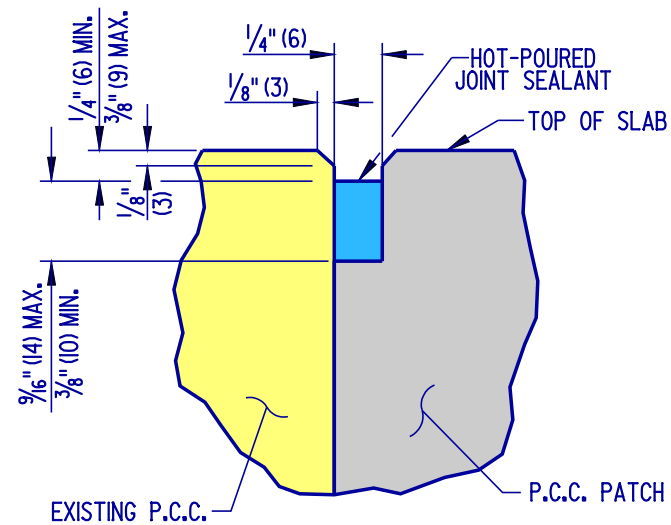
EXIST. P.C.C. PAVEMENT

SECTION C-C

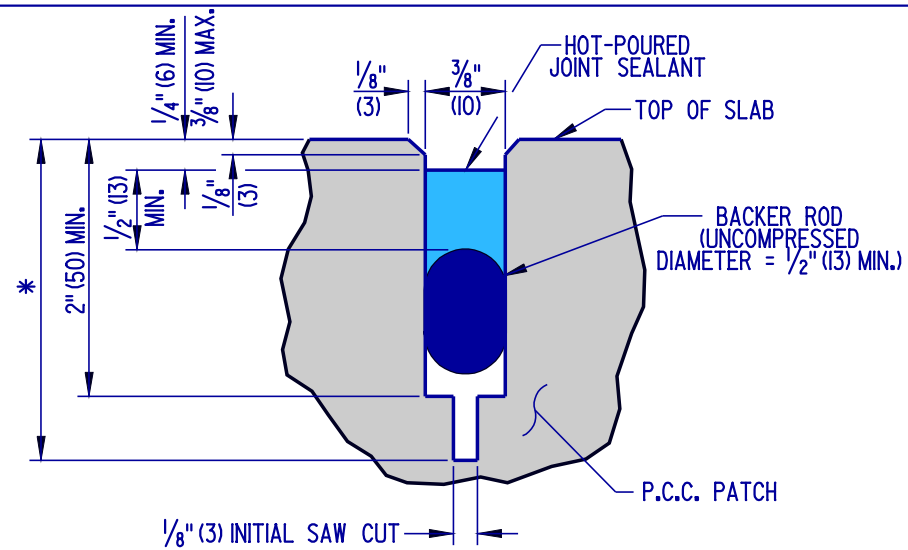
TRANSVERSE CONSTRUCTION JOINT USED ON
JOINTS BETWEEN EXISTING PAVEMENT AND PATCH

FULL DEPTH PATCH

 DELAWARE DEPARTMENT OF TRANSPORTATION	P.C.C.PAVEMENT PATCHING			APPROVED  11/18/08 CHIEF ENGINEER DATE
	STANDARD NO. P-2 (2008)	SHT. 2 OF 5		RECOMMENDED  11/17/08 DESIGN ENGINEER DATE

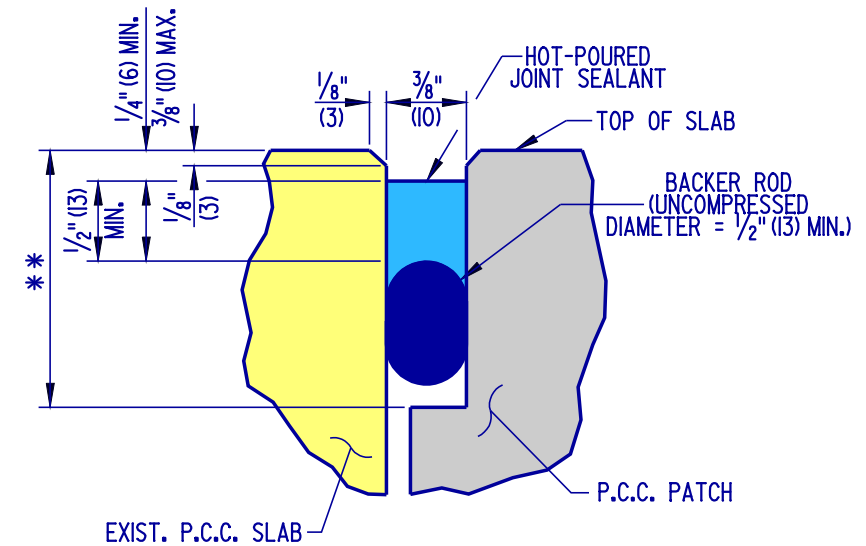


**SEALANT DETAIL-
LONGITUDINAL JOINT**



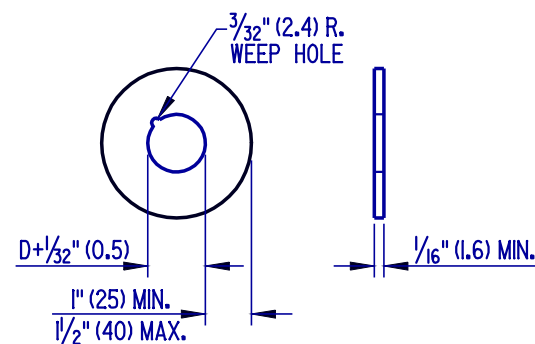
* - 0.3T (T < 10" (250) P.C.C. PAVEMENT)
0.4T (T > 10" (250) P.C.C. PAVEMENT)

**SEALANT DETAIL-
TRANSVERSE SAW-CUT JOINT**



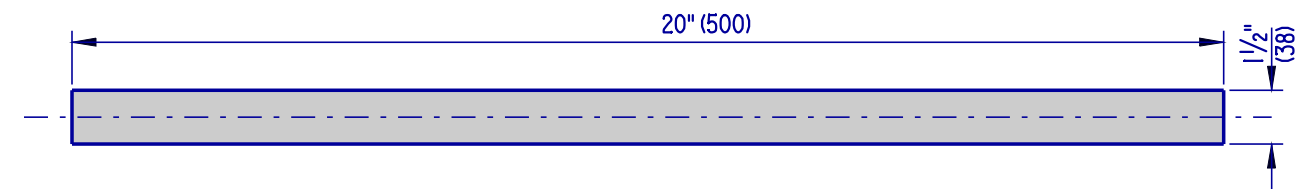
** - 2" (50) MIN. WITH BACKER ROD
5/8" (16) MIN. WITH BOND BREAKER TAPE

**SEALANT DETAIL-
TRANSVERSE CONSTRUCTION JOINT**



D - DOWEL DIAMETER (INCLUDING
PROTECTING COATINGS, IF ANY.)

GROUT RETENTION DISK



DOWEL BAR

NOTES:

1. AS DIMENSIONED, THE WIDTH OF THE TRANSVERSE SEALANT RESERVOIR IS APPLICABLE WHEN THE TEMPERATURE OF THE PAVEMENT SURFACE IS BETWEEN 60°F (16°C) AND 80°F (27°C). WHEN THE TEMPERATURE IS BELOW 60°F (16°C), THE SEALANT RESERVOIR SHALL BE CUT 1/16" (2) WIDER. WHEN THE TEMPERATURE IS ABOVE 80°F (27°C), THE SEALANT RESERVOIR SHALL BE CUT 1/16" (2) NARROWER.
2. "T" REFERS TO THE EXISTING "AS-BUILT" SLAB THICKNESS.
3. TOLERANCE ON ALL JOINT SEALANT DETAIL DIMENSIONS SHOWN WITHOUT RANGES SHALL BE PLUS 1/16" (2), MINUS 0" (0).
4. THE TOP EDGES OF THE CONTACT SURFACES OF THE SEALANT MATERIAL ON BOTH SIDES OF THE JOINT RESERVOIR SHALL BE AT THE SAME ELEVATION.

FULL DEPTH PATCH



**DELAWARE
DEPARTMENT OF TRANSPORTATION**

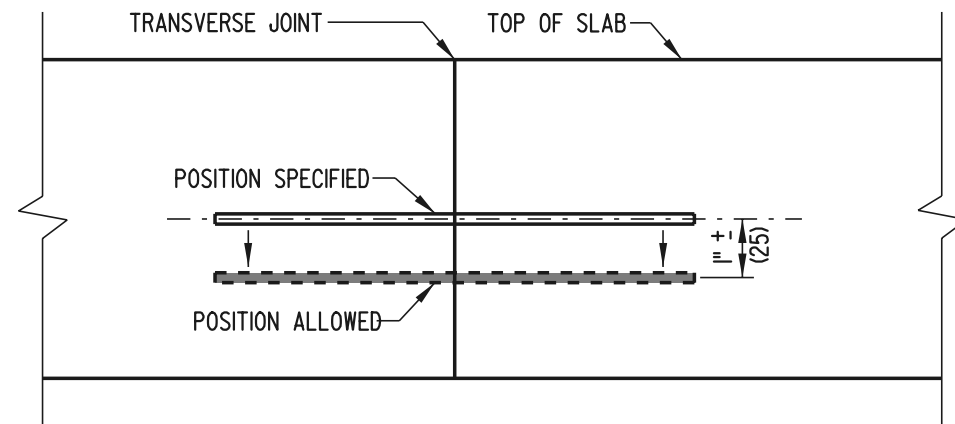
P.C.C. PAVEMENT PATCHING

STANDARD NO. P-2 (2004)

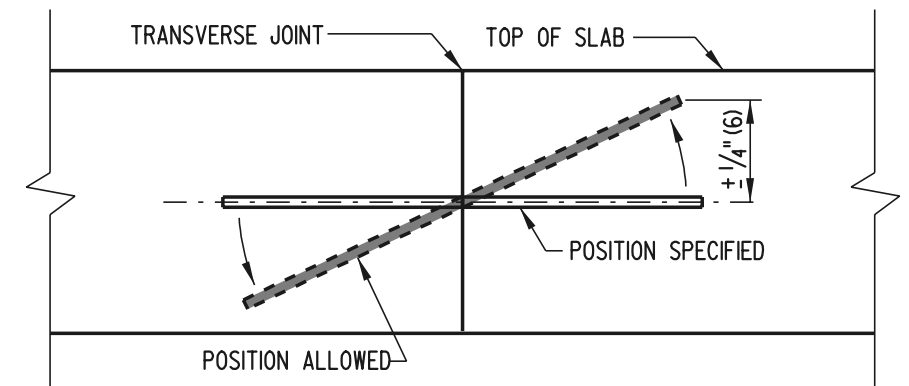
SHT. 3 OF 5

APPROVED *Carolann Wicks* **1/10/05**
CHIEF ENGINEER DATE

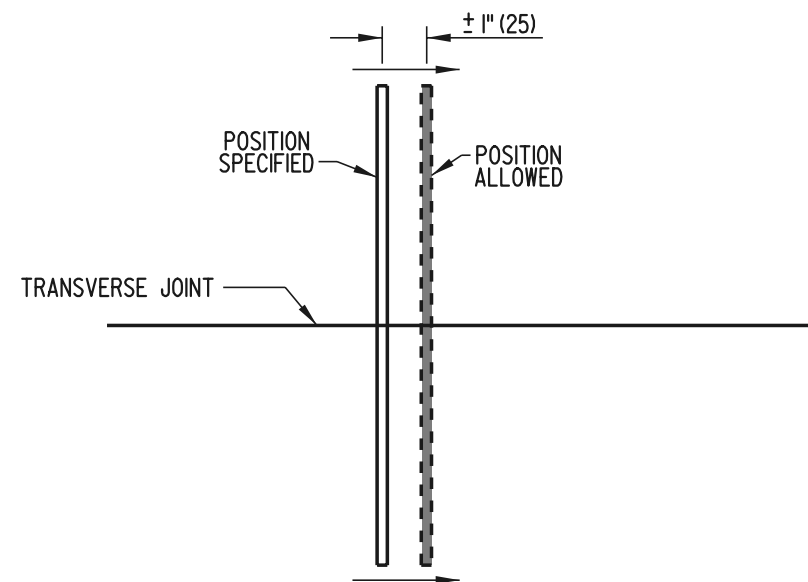
RECOMMENDED *Dennis M. O'Flaherty* **1/13/05**
DESIGN ENGINEER DATE



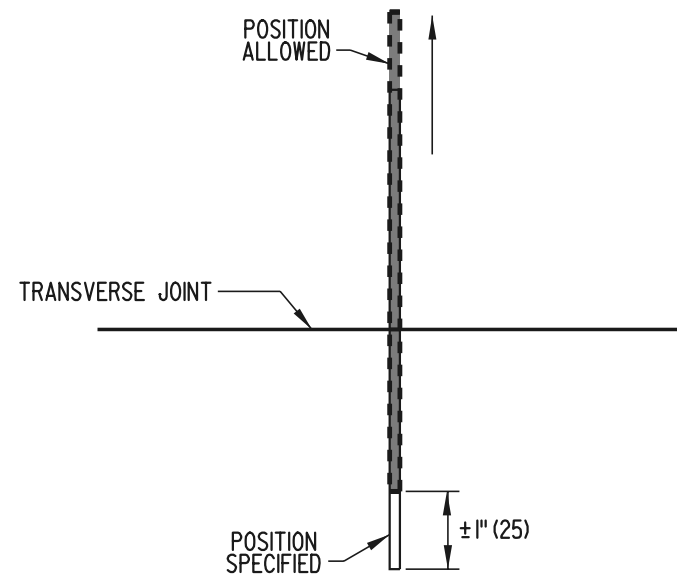
VERTICAL TRANSLATION



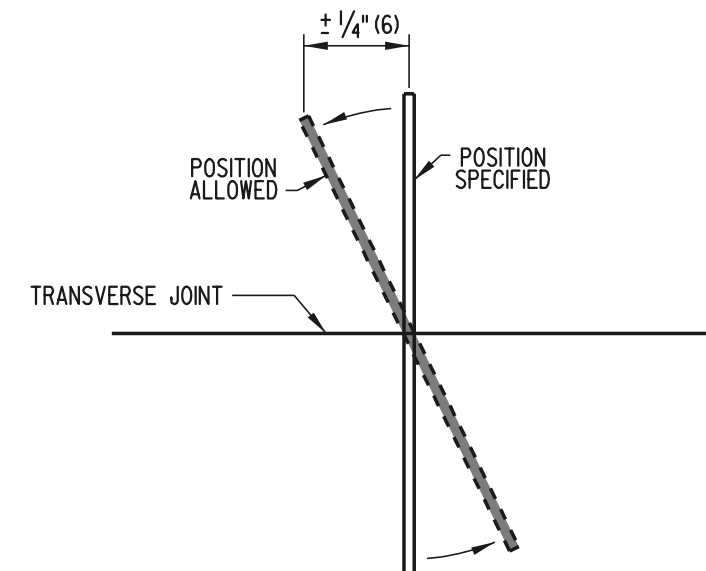
VERTICAL ROTATION



HORIZONTAL TRANSLATION



LONGITUDINAL TRANSLATION



HORIZONTAL ROTATION

DOWEL & TIE BAR PLACEMENT TOLERANCES

FULL DEPTH PATCH



DELAWARE
DEPARTMENT OF TRANSPORTATION

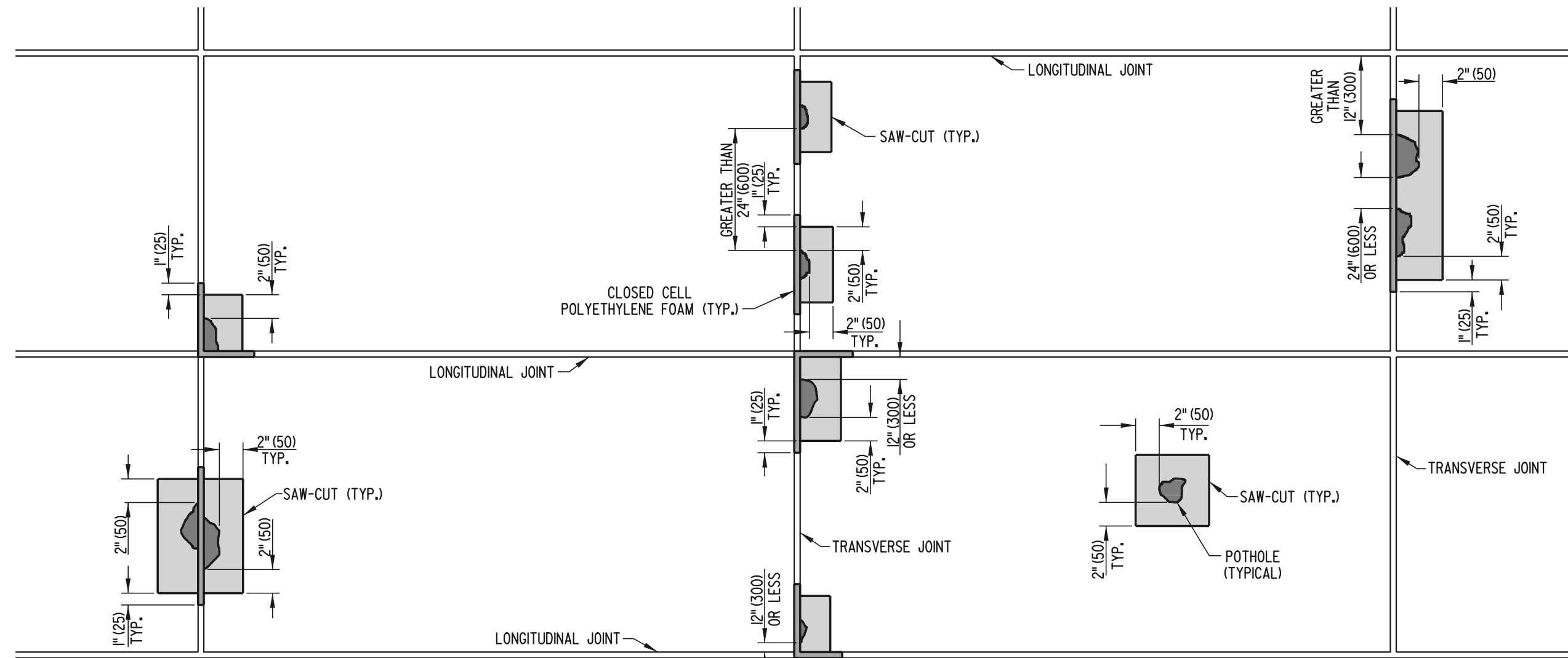
P.C.C. PAVEMENT PATCHING

STANDARD NO. P-2 (2001)

SHT. 4 OF 5

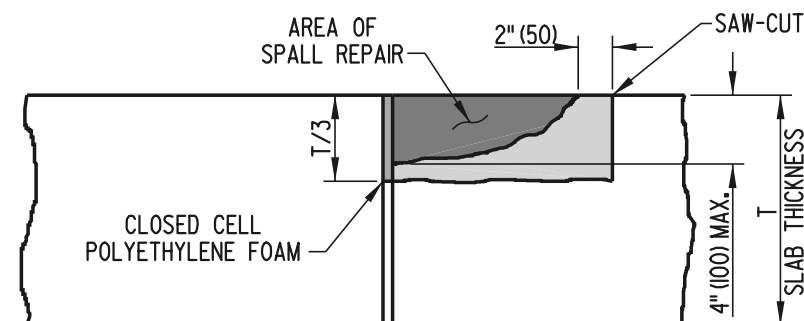
APPROVED *Ryan M. Harkness* 6/18/01
CHIEF ENGINEER DATE

RECOMMENDED *Michael P. Gotsch* 6/18/01
DESIGN ENGINEER DATE

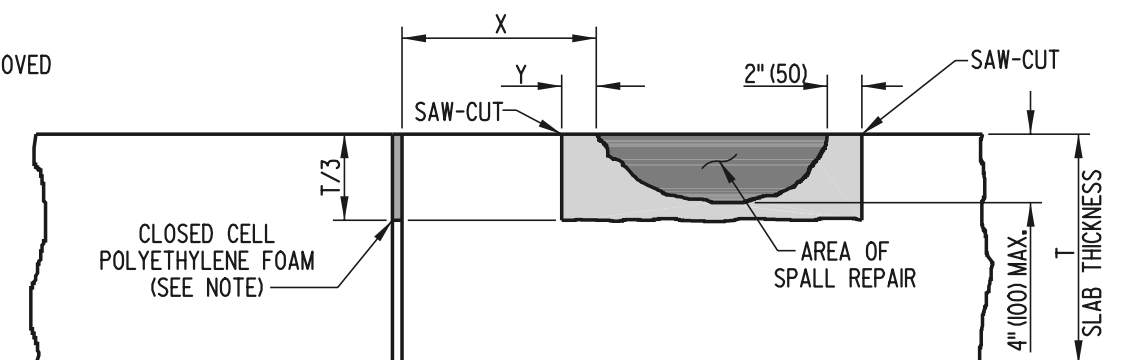


PLAN

NOTE: CLOSED CELL POLYETHYLENE FOAM SHALL BE THE SAME WIDTH AS THE JOINT AND 5"(125) IN DEPTH. AFTER THE CONCRETE IN THE REPAIR AREA HAS ACHIEVED THE SPECIFIED STRENGTH, THE FOAM SHALL BE REMOVED AND REPLACED WITH BACKER ROD AND HOT-POUR SEALANT MEETING ALL APPLICABLE STANDARD DETAILS AND SPECIFICATIONS.

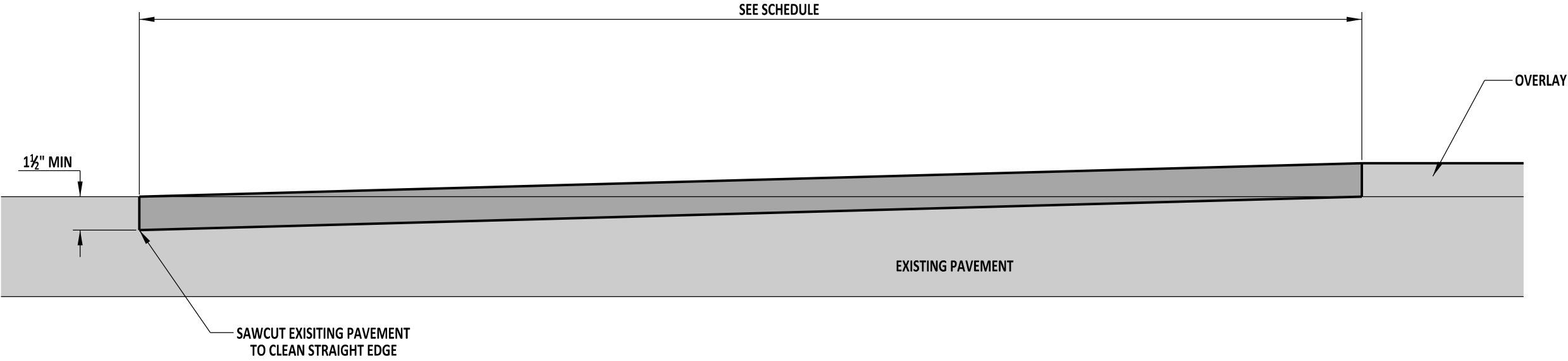


SECTION WITH SPALL ADJACENT TO JOINT



SECTION WITH SPALL NOT ADJACENT TO JOINT

NOTE: WHEN $X > 12"$ (300), THEN $Y=1"$ (25) AND POLYETHYLENE FOAM IS NOT USED.
WHEN $X \leq 12"$ (300), THEN $Y=X$ AND POLYETHYLENE FOAM IS USED.



NOTE:
THE PROFILE OF THE OVERLAY PAVING SHALL BE ADJUSTED TO ASSURE A SMOOTH TRANSITION THROUGH THE BUTT JOINT.

CONDITION	SLOPE
GREATER THAN OR EQUAL TO 55 MPH	40:1
LESS THAN 55MPH	30:1
STOP OR INTERSECTION	15:1



DELAWARE
DEPARTMENT OF TRANSPORTATION

BUTT JOINTS

STANDARD NO.

P-3 (2012)

SHT.

1

OF

1

APPROVED

SIGNATURE ON FILE
CHIEF ENGINEER

01/07/2013
DATE

RECOMMENDED

SIGNATURE ON FILE
DESIGN ENGINEER

12/20/2012
DATE