

PEDESTRIAN CONNECTION TYPE 1, 2, 3, OR 4 (FOR ILLUSTRATIVE PURPOSES) SEE APPROPRIATE DETAIL

SEE PLANS FOR CURB TYPE

2'-0\"/>

DETECTABLE WARNING TRUNCATED DOMES (SEE NOTES 10 & 11)

LANDING, CROSS SLOPE AND RUNNING SLOPE 50:1 MAX

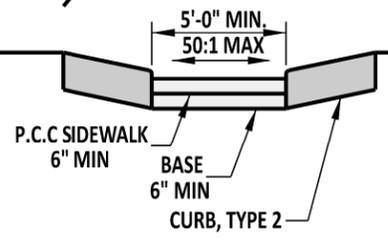
TURNING SPACE, CROSS SLOPE AND RUNNING SLOPE 50:1 MAX

LANDING, CROSS SLOPE AND RUNNING SLOPE 50:1 MAX

DETECTABLE WARNING TRUNCATED DOMES (SEE NOTES 10 & 11)

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SECTION E-E

NOTES:

- 1). FOR ALTERATIONS WHERE THE MAXIMUM ALLOWABLE 12:1 RUNNING SLOPE WILL NOT MEET THE EXISTING SIDEWALK GRADE WITHIN A LENGTH OF 15'-0", THE SLOPED SEGMENT OF THE PEDESTRIAN CONNECTION MAY BE LIMITED TO 15'-0" AT A CONSTANT SLOPE, AND ALLOWED TO EXCEED THE 12:1 MAXIMUM SLOPE.
- 2). PEDESTRIAN CONNECTION AND SIDEWALK CROSS SLOPE SHALL BE 50:1 (2%) MAXIMUM. FOR REHABILITATION WORK, THE PEDESTRIAN CONNECTION CROSS SLOPE MAY MATCH THE SLOPE OF THE ADJACENT ROADWAY IN ACCORDANCE WITH THE LATEST VERSION OF THE PAS MANUAL.
- 3). THE MAXIMUM ALGEBRAIC DIFFERENCE IN GRADE BETWEEN THE PEDESTRIAN CONNECTION OR MODIFIED CURB AT THE FLOW LINE AND THE PAVEMENT SHALL BE 13.3%, HOWEVER 11% IS PREFERRED.
- 4). LANDING AREA SHALL BE CLEARLY DELINEATED WITH JOINTS.
- 5). REFER TO THE DELAWARE MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES FOR DETAILS REGARDING THE LOCATION OF PEDESTRIAN PUSH BUTTONS.
- 6). CONSTRUCTION JOINTS ARE REQUIRED ON PEDESTRIAN CONNECTIONS AT THE INTERVAL SPECIFIED IN NOTE 6 ON DETAIL M-3, SHEET 1 OF 1. HOWEVER, EXPANSION MATERIAL SHALL NOT BE USED IN THE PEDESTRIAN CONNECTION SECTION.
- 7). IF THE RUNNING SLOPE IS LESS THAN 20:1 (5%) THEN THE 50:1 (2%) LANDING CAN BE OMITTED. DETECTABLE WARNING SYSTEM MUST STILL BE PLACED.
- 8). IN ISLANDS AND MEDIANS, A CONTINUOUS PATH WITH A MAXIMUM RUNNING SLOPE OF 20:1 (5%) MUST BE PROVIDED BETWEEN PEDESTRIAN CONNECTIONS. AN INTERMEDIATE LANDING CONSISTING OF A 5'-0" BY 5'-0" WITH A MAXIMUM RUNNING SLOPE AND CROSS SLOPE OF 50:1 (2%) IS REQUIRED ONLY IN LOCATIONS WHERE THE PEDESTRIAN CONNECTIONS INTERSECT BEFORE REACHING FULL HEIGHT.
- 9). A CUT-THROUGH LEVEL WITH THE STREET IS THE PREFERRED TREATMENT FOR ISLANDS. RAMPS OR BLENDED TRANSITIONS CAN BE USED WHERE THE ISLAND IS OF SUFFICIENT SIZE TO ACCOMODATE THEM. POSITIVE DRAINAGE MUST BE PROVIDED FOR EITHER TREATMENT. EITHER TREATMENT IS ACCEPTABLE.
- 10). WHERE THERE IS NO DEPRESSED CURB AT A CUT-THROUGH OR PEDESTRIAN CONNECTION, THE DETECTABLE WARNING SHALL BE INSTALLED 3" FROM THE PAVEMENT EDGE. WHERE THERE IS DEPRESSED CURB, THE DETECTABLE WARNING SYSTEM SHALL BE INSTALLED DIRECTLY BEHIND THE FULL WIDTH OF THE DEPRESSED CURB.
- 11). DETECTABLE WARNINGS SHALL BE INSTALLED WHEN THE LENGTH W IN THE DIRECTION OF PEDESTRIAN TRAVEL IS 6'-0" OR GREATER.



DELAWARE
DEPARTMENT OF TRANSPORTATION

PEDESTRIAN CONNECTION, TYPE 5 & SECTIONS			
STANDARD NO.	C-2 (2018)	SHT.	3 OF 3

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