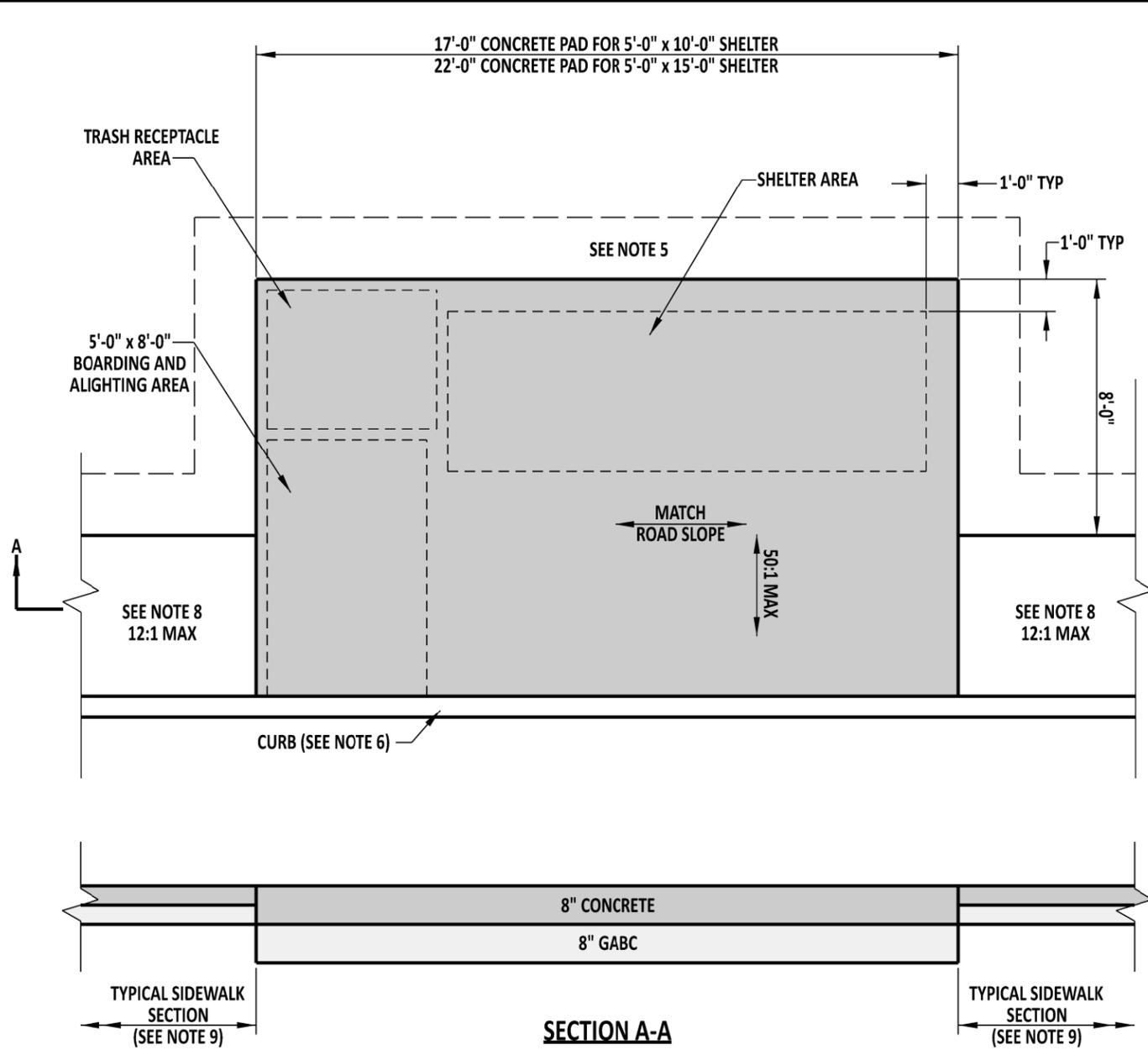
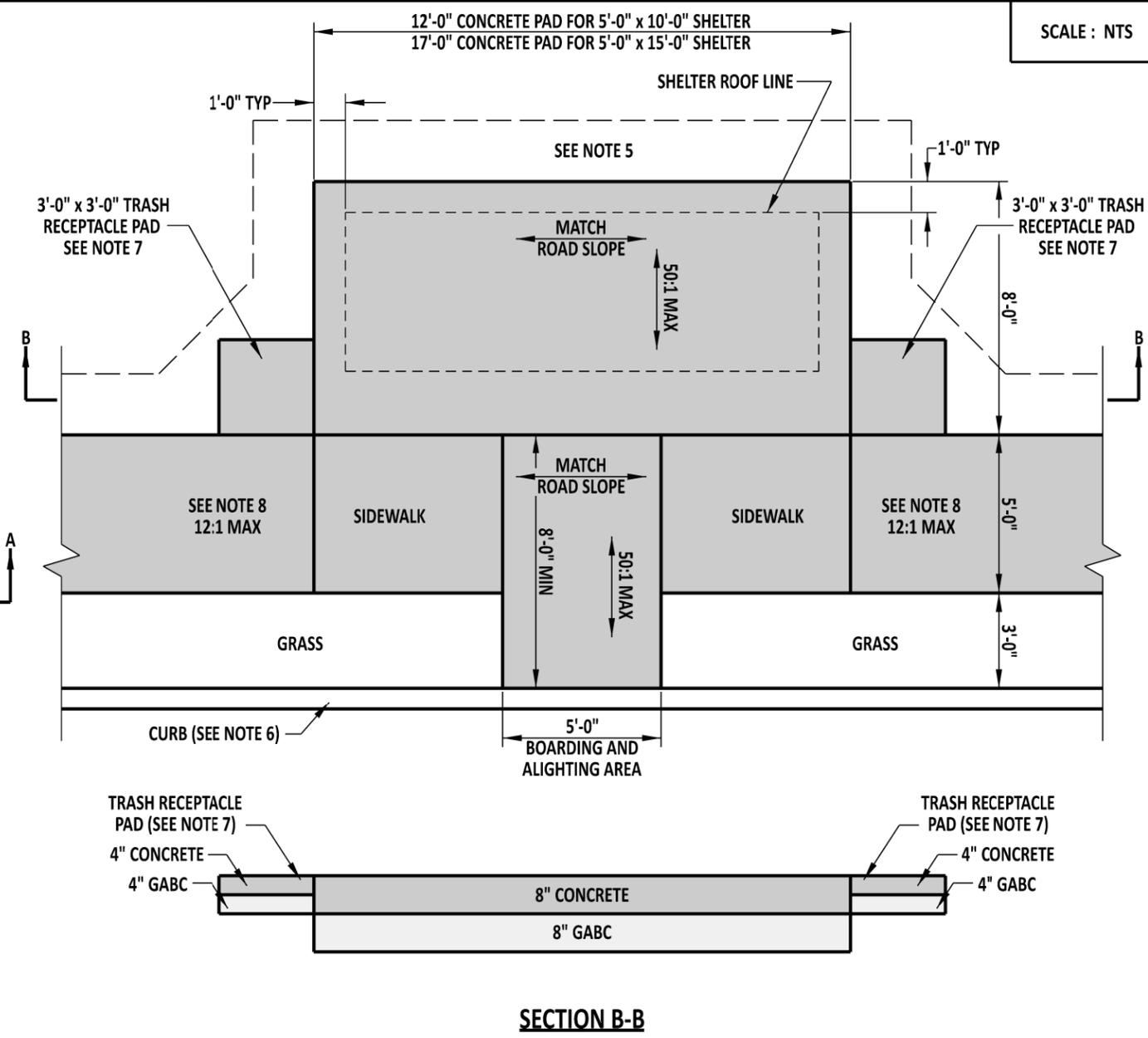


SCALE : NTS



BUS STOP WITH SHELTER PAD, TYPE 1



BUS STOP WITH SHELTER PAD, TYPE 2

- NOTES:**
- 1). BUS STOP SHELTER PAD LOCATIONS MUST BE APPROVED BY DART AND DELDOT PRIOR TO ANY CONSTRUCTION.
 - 2). REFERENCE THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR GENERAL INFORMATION ON PLACEMENT OF SIGNS.
 - 3). SEE CONSTRUCTION PLANS SIGNING AND STRIPING SHEETS FOR SPECIFIC SIGN AND SIGN LOCATION DETAILS
 - 4). BUS STOP CONFIGURATIONS MAY VARY DUE TO TOPOGRAPHIC OBSTRUCTIONS OR GRADES. CONSULT DART OR DELDOT FOR OPTIONAL PAD DETAILS.
 - 5). A 6:1 MAX SLOPE IS REQUIRED FOR 2'-0" ON ALL SIDES OF THE BUS STOP PAD AND APPROACHING SIDEWALKS.
 - 6). CURB TYPE VARIES. SEE PLANS FOR CORRECT CURB TYPE.
 - 7). TRASH RECEPTACLE PAD CAN BE PLACED ON EITHER SIDE OF THE SHELTER PAD, AT THE DIRECTION OF THE ENGINEER IN THE FIELD.
 - 8). THE RUNNING SLOPE TO TRANSITION THE SIDEWALK TO MEET BUS STOP ELEVATION IS 12:1 (8.3%), HOWEVER, 20:1 (5%) IS PREFERRED.
 - 9). SEE DETAIL M-3, SHEET 1 FOR ADDITIONAL SIDEWALK DETAIL.



DELAWARE
DEPARTMENT OF TRANSPORTATION

BUS STOP PAD WITH SHELTER DETAILS

STANDARD NO. M-9 (2018) SHT. 2 OF 2

APPROVED

SIGNATURE ON FILE
CHIEF ENGINEER 1/04/2019
DATE

RECOMMENDED

SIGNATURE ON FILE
DESIGN ENGINEER 12/20/2018
DATE