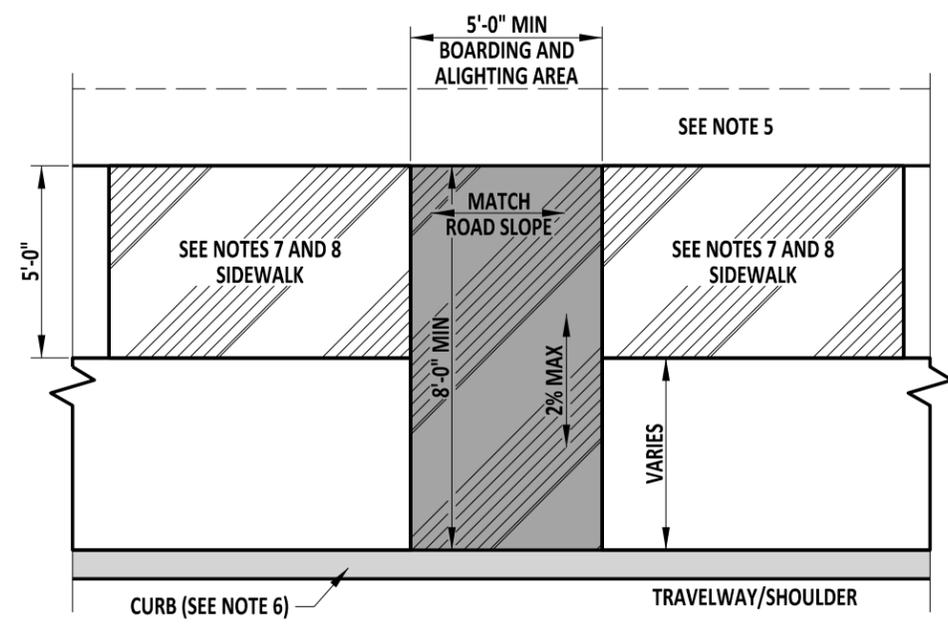


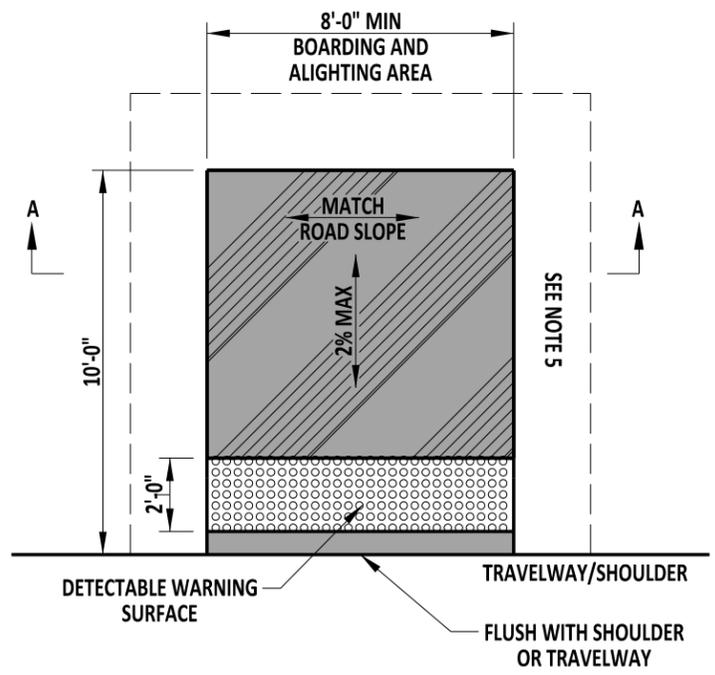
**BUS STOP PAD, TYPE 1**

\* - TO BE USED WHEN THE PAD IS PLACED BEHIND CURB AND INCLUDES A SIDEWALK WITHOUT A GRASS STRIP



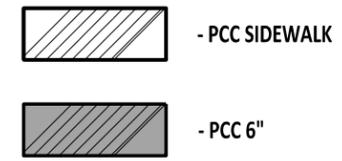
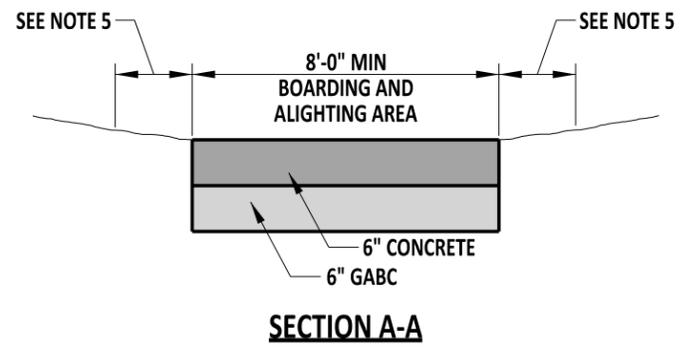
**BUS STOP PAD, TYPE 2**

\* - TO BE USED WHEN THE PAD IS PLACED BEHIND CURB AND INCLUDES A SIDEWALK WITH A GRASS STRIP



**BUS STOP PAD, TYPE 3**

\* - TO BE USED WHEN THE PAD IS PLACED FLUSH WITH THE TRAVELWAY AND NO CURB OR SIDEWALK IS INCLUDED



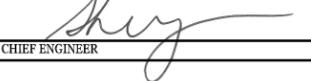
**NOTES:**

- 1). BUS STOP PAD LOCATIONS TO BE APPROVED BY BOTH DART AND DELDOT PRIOR TO ANY CONSTRUCTION.
- 2). REFERENCE THE DE MUTCD FOR GENERAL INFORMATION ON PLACEMENT OF SIGNS.
- 3). SEE CONSTRUCTION PLAN SIGNING AND STRIPING SHEETS FOR SPECIFIC SIGN AND SIGN LOCATION DETAILS.
- 4). TYPICAL BUS STOP PADS MAY BE USED IN CONJUNCTION WITH BUS STOP SHELTER LOCATIONS IN THE EVENT OF LAND CONSTRAINTS AT THE SHELTER LOCATIONS. AN INTERCONNECTING PEDESTRIAN ACCESS PATH MUST EXIST THAT IS ACCESSIBLE TO BUS STOP ALIGHTING AREAS, SHELTERS, PEDESTRIAN CONNECTIONS, CROSSWALKS, AND SIDEWALKS.
- 5). A 6:1 GRADE IS REQUIRED FOR A MINIMUM OF 2'-0" IMMEDIATELY ADJACENT TO THE BUS STOP PAD OR APPROACHING SIDEWALK.
- 6). SEE PLANS FOR CORRECT CURB TYPE. DO NOT DEPRESS CURB
- 7). SEE DETAIL M-3, SHEET 1 OF 1 FOR ADDITIONAL SIDEWALK DETAILS AND REQUIREMENTS.
- 8). THE MAXIMUM RUNNING SLOPE TO TRANSITION THE SIDEWALK TO MEET BUS STOP PAD ELEVATION IS 12:1 (8.3%), HOWEVER, 20:1 (5%) IS PREFERRED. RAMPS ARE ONLY REQUIRED WHEN THE VERTICAL HEIGHT OF THE APPROACHING SIDEWALK DIFFERS FROM THAT OF THE ADJACENT CURB.



  
 ENGINEERING SUPPORT      DATE 09/01/2020  
**RECOMMENDED**

**BUS STOP PAD, TYPES 1, 2 & 3**  
 STANDARD NO.      M-9 (2020)      SHT. 1 OF 2

REVIEWED            DATE 09/01/2020  
 APPROVED            DATE 09/01/2020